

Village of Algonquin

Downtown Streetscape Master Plan



July 20, 2016

**THE
LAKOTA
GROUP.**



Acknowledgements

VILLAGE BOARD

John Schmitt	Village President
Brian Dianis	Village Trustee
Jerry Glogowski	Village Trustee
Robert Smith	Village Trustee
Debby Sosine	Village Trustee
John Spella	Village Trustee
Jim Steigert	Village Trustee

VILLAGE STAFF

Tim Schloneger	Village Manager
Russell Farnum	Community Development Director
Shawn Hurtig	Project Manager
Vince Kilcullen	Streets Supervisor
Steve Ludwig	General Services Superintendent
Ben Mason	Senior Planner
Robert Mitchard	Public Works Director
Katie Parkhurst	Senior Planner
Andy Warmus	Utilities Superintendent
Michele Zimmerman	Assistant Public Works Director

Table of Contents

- PROJECT OVERVIEW 5
- EXISTING CONDITIONS 6
- EXISTING MATERIALS PALETTE 8
- HIERARCHY & ANALYSIS 9
- MATERIALS 11
- PRIMARY STREETS 16
- SECONDARY STREETS 20
- TERTIARY STREETS 24
- SPECIALTY FEATURES 30
- PROJECT STAGING 39
- PRELIMINARY COSTS 40



STUDY AREA

The Downtown Streetscape Master Plan seeks to create a unified vision for the streetscape within the established Downtown/ Main Street Area (see adjacent figure) that can be accomplished through a phased implementation strategy over the next several years. The goal of the streetscape program is to create an attractive, unique and walkable environment that supports the existing mixed-use, historic Downtown. The streetscape program should consider high quality and easily maintainable materials that have a low life-cycle cost.

Project Overview

The Village of Algonquin hired the team of Christopher B. Burke Engineering Ltd. (CBBEL) and The Lakota Group Inc. (Lakota) in June of 2015 to prepare a Downtown Streetscape Master Plan. These two firms conducted a nine-month process to develop a framework to guide the physical improvement to the public rights-of-way within the Village's downtown.

This document represents the summary of this process with the following goals:

- Establish a unified design for streetscape in the downtown and set of materials that can be implemented in a phased approach
- Highlight and enhance the historic character of the downtown
- Create additional visual character and amenities to attract and retain shoppers
- Improve pedestrian comfort and circulation
- Introduce traffic calming features to slow traffic
- Incorporate bicycle lanes and amenities
- Better announce the downtown at the by-pass and along Algonquin Road
- Improve wayfinding to and within the downtown including to convenient parking

Streetscape Master Plan Process

The nine month process included the following key steps and meetings:

- Lakota/CBBEL hired for Downtown/Main Street Streetscape Master Plan - June 2015
- Kick-off Meeting and Walking Tour with Village - July 2015
- Preliminary Hierarchy and Material Discussion - September 2015
- Coordination with Historic Commission - September 2015
- Preliminary Streetscape and Amenity Concepts with Village Staff - November 2015
- Refined Streetscape and Amenity Concepts with Village Staff and Officials - December 2015
- Refined Plan and Preliminary Costs with Village Staff and Officials - February 2016
- Presentation to the Committee of the Whole - March 2016
- Additionally, the Streetscape Master Plan was presented at an open house for local business and property owners in the downtown - May 2016

PREVIOUS STUDIES

The consultant team reviewed previous plans and documents to ensure the process built upon the key recommendations from these studies, including:

- The Village of Algonquin Comprehensive Plan, March 2008
- Downtown Planning Study, March 2013

Existing Conditions

As part of the Master Plan process, the existing streetscape character was reviewed and analyzed, including existing materials, adjacent land uses, sidewalk widths, parking locations, view-sheds, access and circulation, and district/community gateways.

As shown in the photos on the following pages, the sidewalk character is a mix of concrete and clay pavers. The pavers are in generally good condition, however there are some areas of differential settling, and there are a few different pavers types found in the downtown area.

The majority of the improvements in this area are within the adjacent park spaces or around the recently completed development bounded by Main Street, Algonquin Road, Harrison Street, and Front Street.

Most of Main Street appears dated, and the roadway is too wide, creating the opportunity for the geometry to be modified to better support pedestrians and bicyclists now that the by-pass has been completed.

The existing amenities do not create an appropriate and comfortable sense of scale - the pedestrian street lights are too short and the street trees are generally small and stunted.



S MAIN STREET



S HARRISON STREET



W ALGONQUIN ROAD



S MAIN STREET



S HARRISON STREET



W ALGONQUIN ROAD BRIDGE



S MAIN STREET



VILLAGE HALL



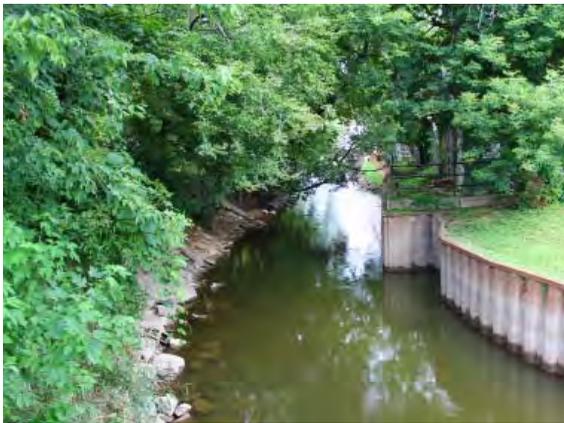
S HARRISON STREET



N HARRISON STREET



W ALGONQUIN ROAD



CRYSTAL LAKE OUTLET AT S HARRISON STREET



CORNISH PARK FROM W ALGONQUIN ROAD BRIDGE



S RIVER ROAD



S RIVER ROAD

Existing Materials Palette

The existing streetscape furniture and amenities were assessed based on condition and upkeep and their overall contribution to Downtown character. Decorative street lights are the most consistent element and help to establish the desired character for the Downtown.

Existing trash receptacles are in good condition and the black decorative metal design compliments the street lighting. Existing benches are in fair to poor condition as many have been damaged by skateboarders and other factors. Additionally, the excessive length of many of the benches detract from their character. Existing newspaper racks/cabinets are utilitarian and do not contribute to the character of the Downtown.



N HARRISON STREET



EXISTING LIGHT POLES



EXISTING LIGHT POLES



EXISTING BENCHES



EXISTING TREE GRATE



EXISTING TRASH RECEPTACLE



EXISTING SIGNAGE



EXISTING SIGNAGE



EXISTING BANNERS

Hierarchy & Analysis

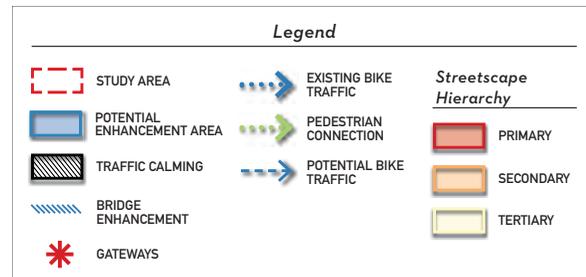
The Main Street/Downtown Streetscape Master Plan is organized around a three tiered hierarchy of the roadway network. This hierarchy was developed through analysis of the existing streetscape. Factors in the analysis included the presence of commercial or retail land use, level of existing pedestrian activity, redevelopment potential of adjacent sites, potential for increased pedestrian activity, opportunities for closing roadway segments for festivals and special events, .

The goal of the hierarchy is to:

- Provide a framework to rationalize the level of cost and complexity of streetscape investments for individual segments of the streetscape network
- Use in developing initial order-of-magnitude cost estimates
- Help organize and structure decision making relative to appropriate prioritization of investment and implementation phasing.

The graphics shown on this and the following page indicate the recommended streetscape hierarchy for the study area.

Additionally, the map highlights other key analysis considerations and opportunities that were used to inform the master plan.



PLAN DATA

- A** POTENTIAL TRAFFIC CALMING
 - Explore speed table or other options
- B** HISTORIC DISTRICT
- C** POTENTIAL FESTIVAL OR SHARED STREET
- D** PEDESTRIAN CROSSING IMPROVEMENTS AT W ALGONQUIN RD
- E** BRIDGE ENHANCEMENTS
 - Improve aesthetics and character
 - Improve pedestrian traffic
- F** POTENTIAL RAMP MODIFICATION TO IMPROVE ACCESS
- G** WALKING TRAIL IMPROVEMENTS
 - Extend walking trail along both sides of Crystal Lake Outlet
 - Provides connection between Towne and Cornish Park and Main Street
- H** POTENTIAL PEDESTRIAN BRIDGE
 - Possible connection from Cornish Park to overlook area
- I** TOWN OVERLOOK
 - Explore options to close portion of S River Rd to Traffic
 - Create clearing in treeline for views of downtown
- J** OPPORTUNITY SITE
 - City property with potential for enhancement
- K** EXPLORE DESIGN OPTIONS FOR CLOSING ROADS DURING SPECIAL EVENTS
- L** EXPLORE OPPORTUNITIES TO REMOVE ON-STREET PARKING & DROPOFFS TO IMPROVE BIKE LANE CONDITIONS
- M** POTENTIAL REAR ACCESS/ALLEY ENHANCEMENTS
- N** POTENTIAL TO ENHANCE VILLAGE HALL GREEN SPACE
- O** SHORTEN CROSSING AT HARRISON
- P** EXPLORE ENHANCEMENT OPPORTUNITIES

PLAN DATA

- P** EXPLORE ENHANCEMENT OPPORTUNITIES
- Q** EXPLORE OPPORTUNITIES TO REDUCE ROAD WIDTH
- R** IMPROVE PEDESTRIAN CONNECTIVITY TO DOWNTOWN

Legend				
	STUDY AREA		EXISTING BIKE TRAFFIC	Streetscape Hierarchy  PRIMARY  SECONDARY  TERTIARY
	POTENTIAL ENHANCEMENT AREA		PEDESTRIAN CONNECTION	
	TRAFFIC CALMING		POTENTIAL BIKE TRAFFIC	
	BRIDGE ENHANCEMENT			
	GATEWAYS			



Materials

Overall

The consultant team, through discussions and input from Village staff, developed a family of streetscape materials. The goals of these materials are to:

- Complement the existing buildings and architecture
- Align with the historic character of the downtown
- Work with the existing furniture found in adjacent parks and developments
- Be durable and low maintenance to achieve optimal life-cycle costs

This family of materials will be applied, where appropriate, to the streetscape enhancements, in alignment with the established hierarchy. The following pages provide the information for each all of the materials selected. At the end of this section, a matrix is provided that identifies the appropriateness of each material or component relative to the three tiers in the hierarchy.

Paving

The selected paving builds upon the existing red clay pavers found throughout the downtown. Where appropriate, similar pavers should be used in the sidewalks to create a vibrant downtown character.

In the primary areas, it is recommended that mostly clay pavers be used in the roadway as it will provide several benefits. It will be important in placemaking and establishing a downtown “district,” it will help with traffic calming, and it will provide a durable roadway material with a low life-cycle cost. The paver should be set in a herringbone pattern as it reduce shifting of the pavers due to traffic.

In these primary areas where pavers are used in the street, the recommended sidewalk material is also clay pavers. However, to create a contrast to the red brick in the roadways, the recommendation is for beige pavers in a range of larger format sizes, laid in a random ashlar pattern. Similar to the roadway, these clay pavers will provide an authentic, durable material that will complement the character of the downtown.

In the secondary areas, the recommendation is for sidewalks to use the red brick pavers in a herringbone pattern. The roadways should be asphalt in these locations.

For tertiary areas, the sidewalks should be scored concrete to minimize cost and maintenance. An attractive, clean and simple score-line pattern should be used in these areas. Similar to the secondary areas, the roadways should be asphalt, and may be limited to resurfacing, depending on existing conditions.



RED CLAY PAVERS SET IN A HERRINGBONE PATTERN



SCORED CONCRETE SIDEWALK



BEIGE CLAY PAVERS SET IN AN ASHLAR PATTERN

Lighting

Lighting within the downtown should build upon the existing lighting and character of the area. The Village has used a traditional acorn light throughout the downtown and adjacent parks and parking lots. As mentioned previously, the lights on Main Street are too short and do not create an appropriate pedestrian scale. A 12' mounting height is recommended. Additionally, the Village's current light standard should be used, which as seen in the parking lot at Towne Park, has a decorative metal top on the acorn light.

The pedestrian light can be customized further to establish a unique Algonquin element through decorative signage. This element could build off of historic details in downtown architecture and help to reinforce the identity of the downtown. It does not need to be placed on every light pole, but should be repeated several times per block face.

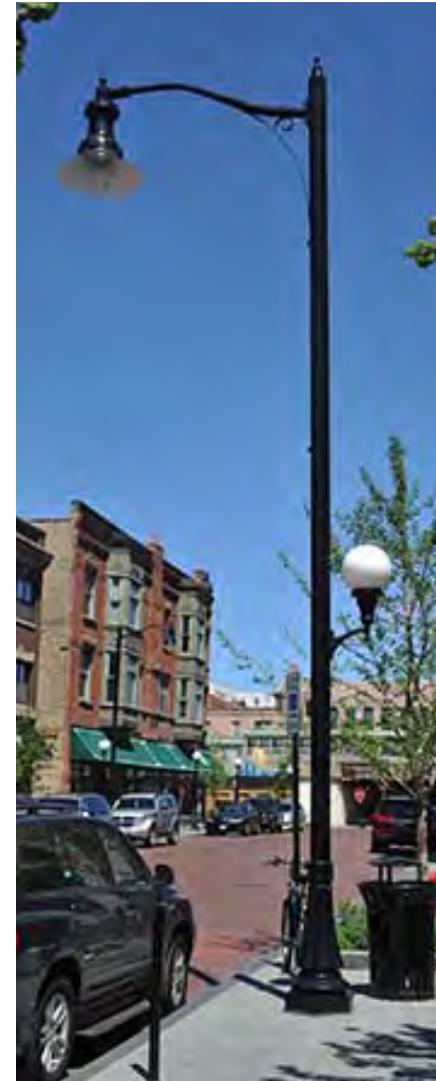
This pedestrian light should be used consistently through the area, especially in locations with heavier pedestrian traffic. However, in some locations, due to the width of the road, or the character of area, an overhead roadway light will be necessary to create nighttime visibility and safety. This light should be a full cut-off fixture that reduces light pollution. It should have a decorative pole and mounting arm to complement the pedestrian lights.



PEDESTRIAN LIGHTING STANDARD AS FOUND IN TOWNE PARK



DOWNTOWN DECORATIVE SIGN CONCEPT (SHOWN ON EXISTING NON-STANDARD POLE)



EXAMPLE OF OVERHEAD ROADWAY LIGHT IN A TRADITIONAL DOWNTOWN STREETScape



OVERHEAD ROADWAY LIGHT CONCEPT

Furniture

Streetscape furniture within the downtown should be durable black metal to match the existing character of the area and the furniture that has been historically used.

As shown in the photo to the right, the downtown had historically used a black metal bench with horizontal slats. The existing bench used in the adjacent parks and downtown has metal ends and a wood seat and back, but is available in horizontal metal slats. This bench would provide a link to the historic character, but still maintain some consistency with established furniture, which will help simplify maintenance for the Village.

For trash receptacles, a decorative metal element is recommended. Based on discussions with staff, a rain shield is preferred for ease of maintenance. Additionally, stand alone trash receptacles should be kept to a minimum, and opportunities to build them into walls or other elements to reduce the visual impact they have on the downtown is preferred.

Bicycle racks provide an opportunity to introduce a decorative and creative element into the downtown. There is already a history of unique bike racks, such as the penny-farthing bike rack on the north side of Main Street, just east of the river. Additionally, there are custom bike racks at Algonquin Commons that have an “A” shape and create unique shadow patterns. Other “A” frame racks, or customized racks could all be used, as opposed to one single standard rack, for an eclectic element.



PHOTO OF HISTORIC DOWNTOWN BENCH



RECOMMENDED METAL BENCH WITH HORIZONTAL SLATS



RECOMMENDED DECORATIVE TRASH RECEPTACLE WITH RAIN SHIELD



CUSTOM BIKE RACK AT ALGONQUIN COMMONS



"A" FRAME BIKE RACK



"A" FRAME BIKE RACK WITH CUSTOM INSERT

Furniture

Tree grates are critical in an urban environment for creating a usable pedestrian surface in the sidewalks while also protecting tree roots and allowing for rainwater to reach the roots. The grate should be as large as reasonable to help create a strong growing environment for the tree, while maintaining an appropriate pedestrian environment.

The design of the tree grate does not need to be overly ornate, but should create some visual interest. The openings should be small enough to ensure the grate does not compromise ADA accessibility or other issues. The design should allow for internal rings to be cut away as the tree grows to provide flexibility and reduce the need to remove or replace the tree grate in the future.

The tree should also be supported by an enhanced growing medium underneath the sidewalks. This can be accomplished through the use of structural soils or Silva Cells, two methods that provide structural support for the sidewalks while allowing more air pockets in the soil for water to infiltrate and roots to grow.

Decorative planter urns can be used to create additional color pockets and seasonal interest in the downtown. These are cast iron and should either be modeled on the planter urns that were historically used in the downtown, some of which have been preserved, or a similar mold should be selected and used. The recommendation is to allow for the cast iron to patina as opposed to painting them.

Another recommended street element is decorative

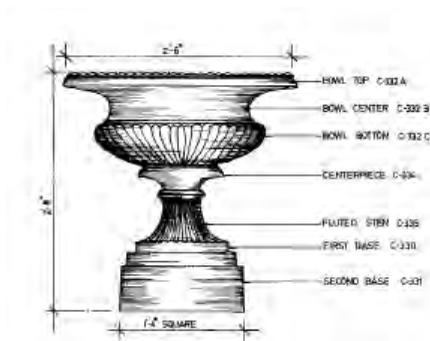


DECORATIVE TREE GRATE



HISTORIC CAST IRON PLANTER URN

stone walls. There are several examples in the adjacent parks. These walls can be incorporated into entry features, help funnel pedestrian traffic, and create opportunities for informal seating.

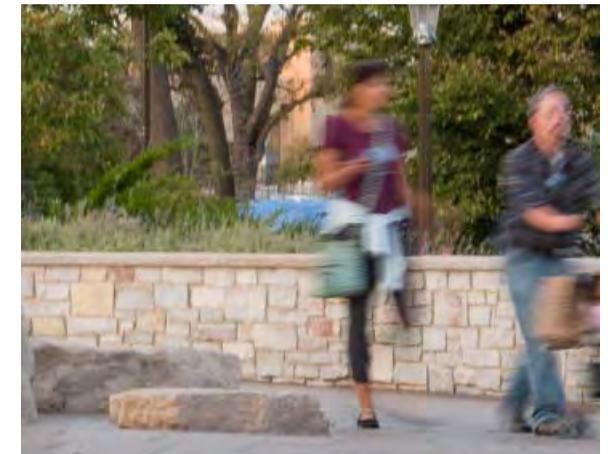


TOWER GARDEN GROVE URN

COMPARABLE CAST IRON PLANTER URN



EXISTING STONE WALL



EXAMPLE OF A STONE SEATWALL

Material Usage Matrix

LEGEND	
Material Frequently Used	
Material Infrequently Used	
Material Not Used	

MATERIALS	PRIMARY STREETS	SECONDARY STREETS	TERTIARY STREETS
RED CLAY PAVERS			
BEIGE CLAY PAVERS			
SCORED CONCRETE			
PEDESTRIAN LIGHTS			
PERMANENT METAL BANNERS			
OVERHEAD ROADWAY LIGHTS			
BENCHES			
TRASH RECEPTACLES			
BICYCLE RACKS			
TREE GRATES			
MOVABLE PLANTERS			
STONE WALLS			

Primary Streets

Main Street - By-pass to Washington

This segment of Main Street includes the entrance from the south at the Illinois Route 31 By-pass to the intersection with Washington Street.

To create a presence at the By-pass, landscaped gateway signs are recommended flanking the entrance to Main Street as well as an overhead lighting gateway element spanning the street.

As a primary segment of the downtown, this portion includes brick paving in the street. The sidewalks are also shown as red brick as there is a

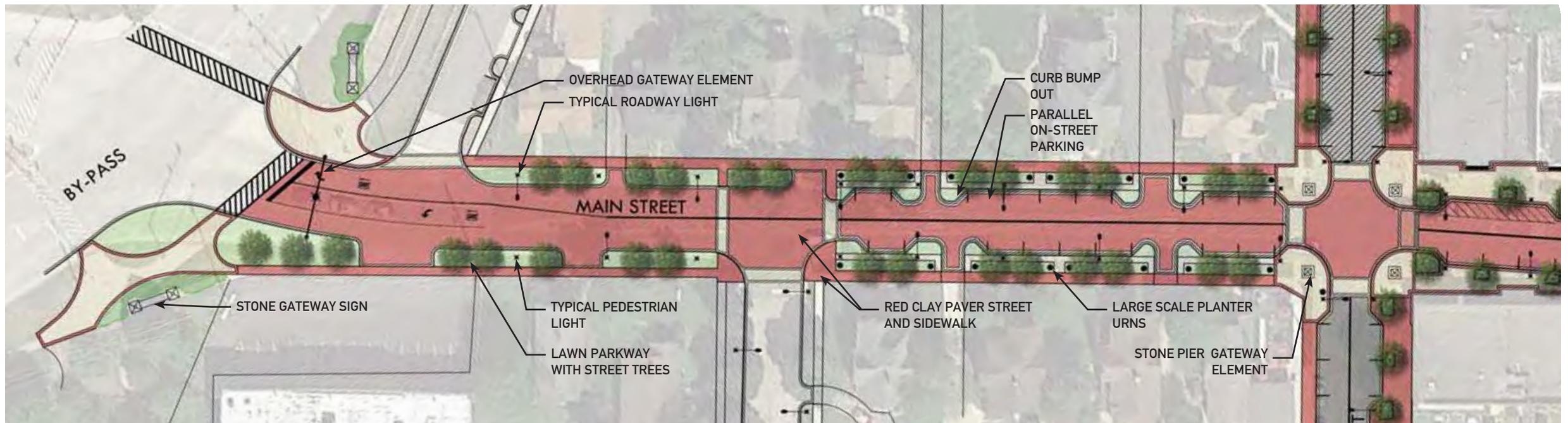
landscaped parkway creating separation from the street, ensuring the paving treatment will not feel monotonous.

The landscaped parkway can act as an extension of the more residential character of the front yards for the businesses in this area, and is envisioned to include flowering perennials and ornamental grasses. Additionally, large planting urns could be used as specialty features in this area.

On-street parking should be maintained to support the local businesses. However, curb bump-outs are encouraged wherever possible to reduce the appearance of road width and create additional areas for landscape.

LEGEND

	Red Clay Pavers		Landscape Enhancement
	Beige Clay Pavers		Grass Parkway
	Scored Concrete		Building
	Resurfaced Asphalt		NORTH
	Reconstructed Asphalt		
	Marked Bike Way		



CONCEPTUAL STREETScape MASTER PLAN - MAIN STREET FROM ILLINOIS ROUTE 31 BY-PASS TO WASHINGTON STREET

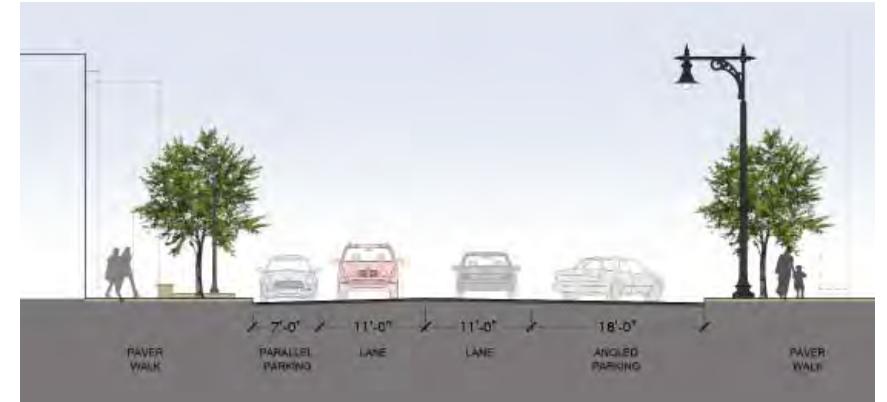
Main Street - Washington to Algonquin

This segment of Main Street should be the core focus of the downtown, with the highest concentration of commercial development, consistent building streetwalls, and historic character.

The geometry of the roadways should formalize and improve the recent re-striping that introduced angle parking on one side of the road. This approach reduces roadway width and slows traffic. Additional improvements can be made to increase sidewalk width and incorporate curb bump-outs for additional landscape.

The treatment for this area should include red clay paver roadways and contrasting beige clay paver sidewalks. There are existing pavers with engraved names that should be salvaged and reinstalled in a key location in the downtown, potentially the plaza adjacent to the old Village Hall.

In addition to decorative lighting and streetscape furniture, there are opportunities to enhance this area with improvements to the bridge over Crystal Creek, to pedestrian alleys, inclusion of stone walls in key locations, and column features at the corners of Main and Washington.



CONCEPTUAL CROSS-SECTION - MAIN STREET BETWEEN WASHINGTON ST AND ALGONQUIN RD.



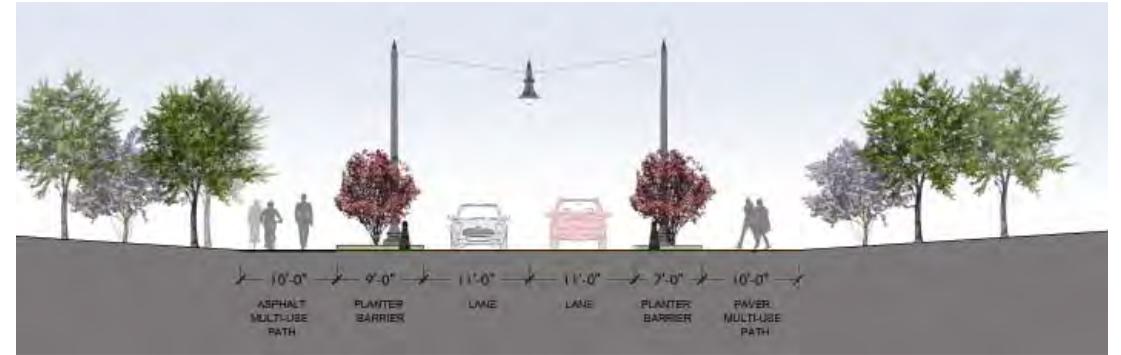
CONCEPTUAL STREETScape MASTER PLAN - MAIN STREET FROM WASHINGTON STREET TO ALGONQUIN ROAD

Harrison Street - Washington to Algonquin

This portion of Harrison Street creates a unique opportunity for both slowing traffic and providing flexibility to use the roadway for festivals. Since it is adjacent to the park, the roadway could function as an extension of the paved surfaces of the park, which could be accomplished through a curbsless road or “shared street.” To appropriately define the vehicular portion of the road, it is separated from the pedestrian and bicycle areas through a series of raised planters between the bridge and Algonquin Road.

This special portion of the road is further highlighted through overhead canopy lights, suspended from poles that are set in the raised planting beds.

The paving treatment follows the primary street palette with red clay pavers in the roadway and beige clay pavers in the sidewalks.



CONCEPTUAL CROSS-SECTION - HARRISON STREET BETWEEN WASHINGTON ST. AND ALGONQUIN RD



CONCEPTUAL STREETScape MASTER PLAN - HARRISON STREET FROM WASHINGTON STREET TO ALGONQUIN ROAD

Washington Street - Harrison to North of Main

This segment of Washington Street extends from Harrison to a point northwest of Main Street and creates an important linkage, tying Main and Harrison together. However, the treatment can vary slightly from the full primary treatment.

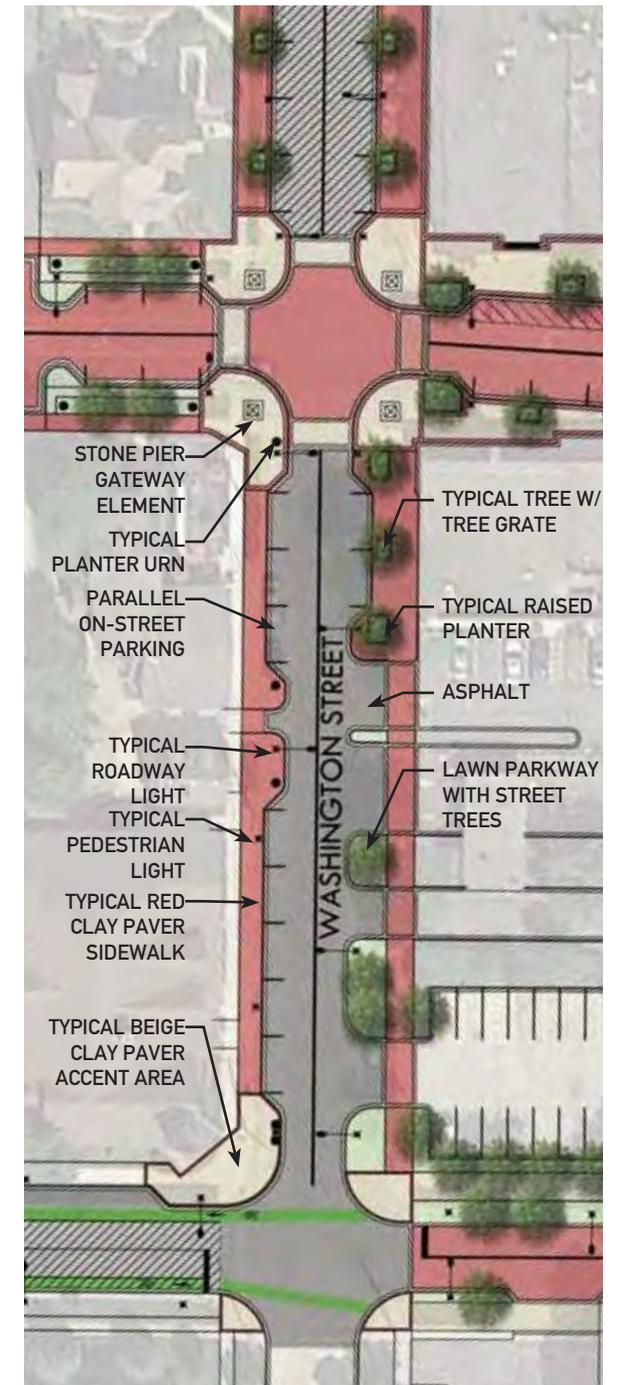
The roadway is recommended to be a new asphalt paving, with red clay paver sidewalks. The roadway geometry should be modified to increase pedestrian sidewalk space and create opportunities for outdoor cafes while maintaining on-street parking. Curb bump-outs should be included to increase opportunities for landscape areas and other enhancements.

If overhead wires remain along Washington, they will limit the opportunities for street trees in some locations.

LEGEND

	Red Clay Pavers		Landscape Enhancement
	Beige Clay Pavers		Grass Parkway
	Scored Concrete		Building
	Resurfaced Asphalt		NORTH
	Reconstructed Asphalt		
	Marked Bike Way		

CONCEPTUAL STREETSCAPE MASTER PLAN - WASHINGTON STREET FROM HARRISON STREET TO MAIN STREET

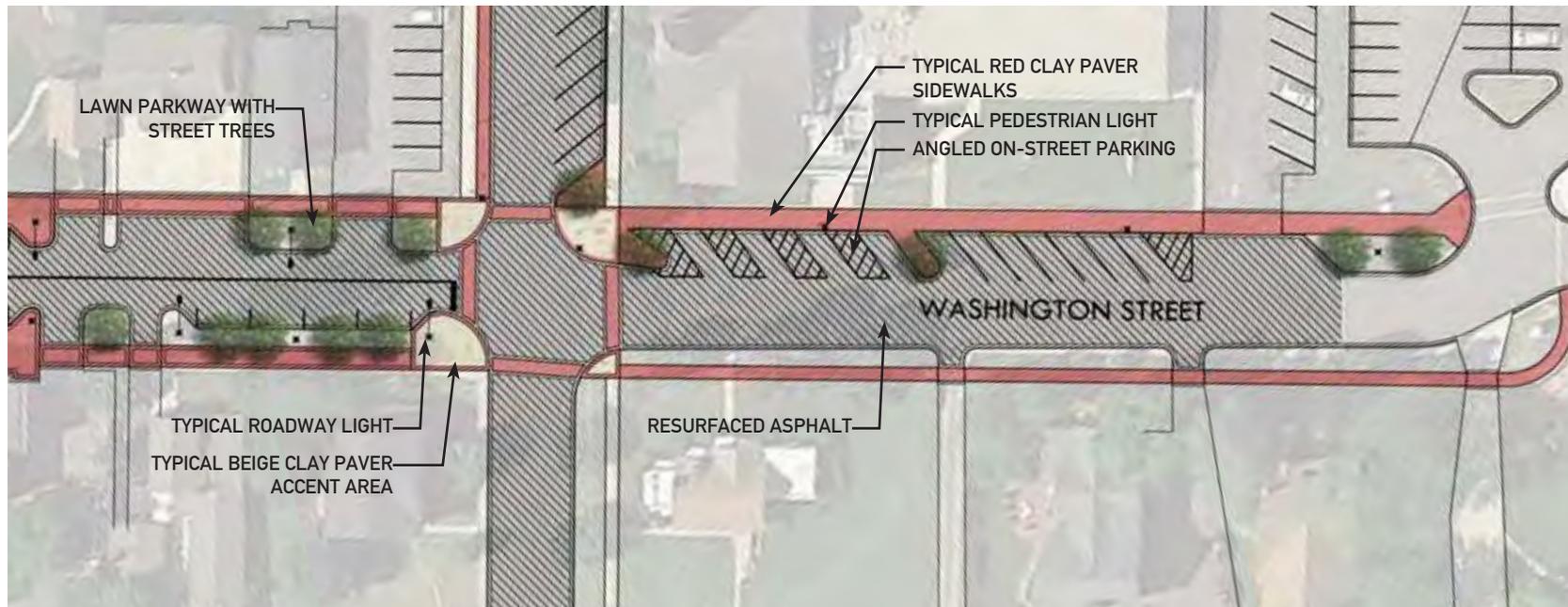


Secondary Streets

Washington Street - Main to Towne Park Parking Lot

Within this area, the sidewalk connections are recommended to be red clay pavers with beige clay paver accents at the corners. Existing pavers in this area should be removed and disposed of as they do not meet the character and quality the Village is looking to achieve. The roadway is recommended for resurfacing.

This section is important as it creates a more appealing pedestrian connection back to the parking lot, which can be better used for downtown parking at peak times, especially if complemented with improved wayfinding signage.



LEGEND

	Red Clay Pavers		Landscape Enhancement
	Beige Clay Pavers		Grass Parkway
	Scored Concrete		Building
	Resurfaced Asphalt		NORTH
	Reconstructed Asphalt		
	Marked Bike Way		

CONCEPTUAL STREETScape MASTER PLAN - WASHINGTON STREET FROM MAIN STREET TO TOWNE PARK PARKING LOT

Algonquin Road - River Rd to Harrison

Improvements to the Algonquin Road bridge into the downtown creates opportunities to address several issues. One is that there is no sense of arrival or announcement of the downtown. A second opportunity is to improve the pedestrian character and comfort of the bridge itself.

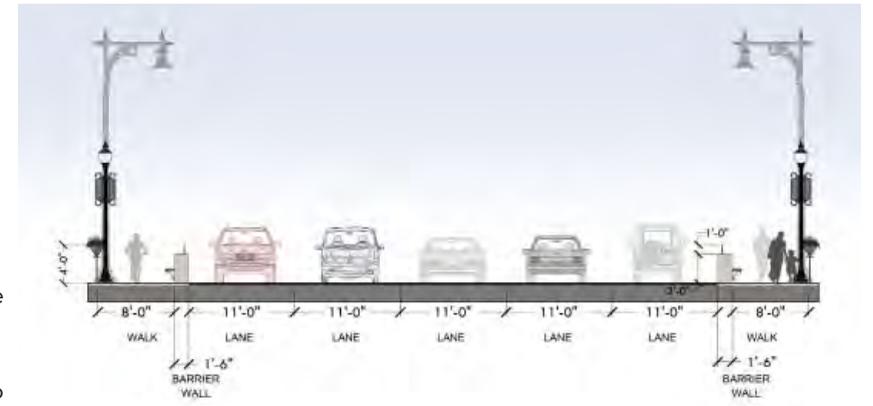
The plan shows entrance monuments at River Road, along with decorative corner paving, to improve the sense of arrival.

The bridge is envisioned as widened to accommodate increased pedestrian space on both

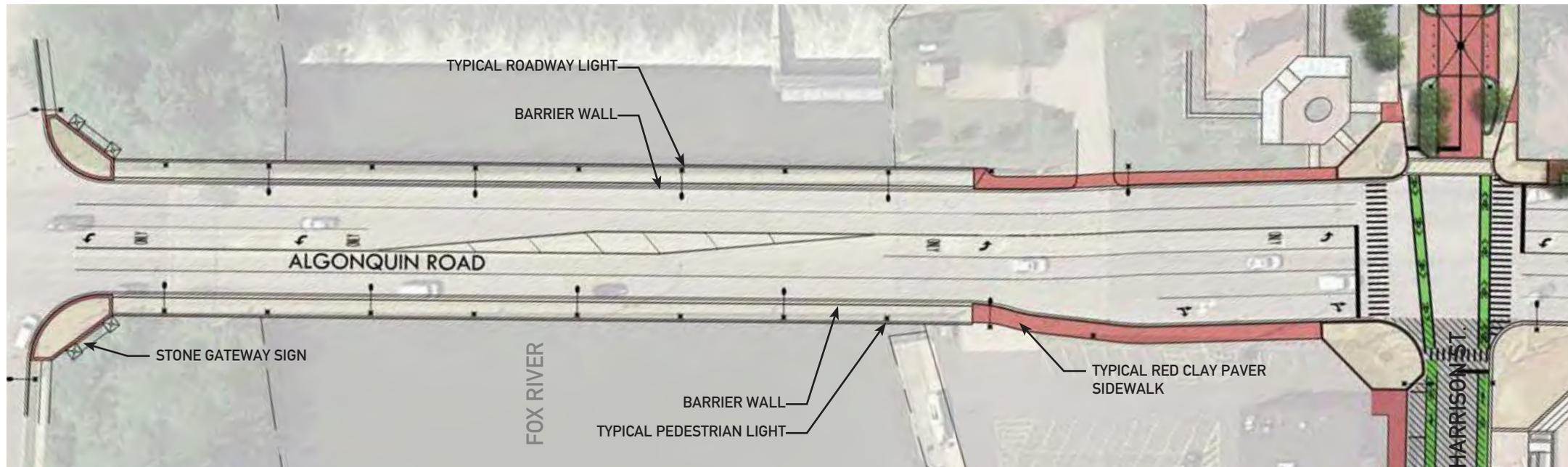
sides. As shown in the cross-section on this page, the pedestrian space is recommended to be 8'-0", separated by a barrier wall from traffic.

Within the pedestrian space, the plan calls for pedestrian scaled lights, as well as decorative overhead lights to illuminate the roadway. A decorative railing should be incorporated on top of the barrier wall and also at the outer edge of the bridge. Hanging planters could be placed on the outer railing to introduce seasonal color and interest to the bridge. Benches could be anchored to the barrier wall at a mid-point of the bridge as well.

While there is some potential to reconfigure the geometry of the bridge to gain additional pedestrian space, the plan as shown would require more intensive changes to the bridge deck. As Algonquin Road is under IDOT jurisdiction, coordination will be required to move this concept forward.



CONCEPTUAL CROSS-SECTION - ALGONQUIN ROAD BRIDGE



CONCEPTUAL STREETScape MASTER PLAN - ALGONQUIN ROAD FROM RIVER ROAD TO HARRISON STREET

Riverside Plaza Block

All of the roadways around the Riverside Plaza development are considered secondary streets, including Algonquin Road, Main Street, Harrison Street and Front Street. While the streetscape immediately adjacent Riverside Plaza is relatively new and should be maintained, the roadways, crosswalks, and streetscape on the opposite sides of the street should be enhanced in line with the plan.

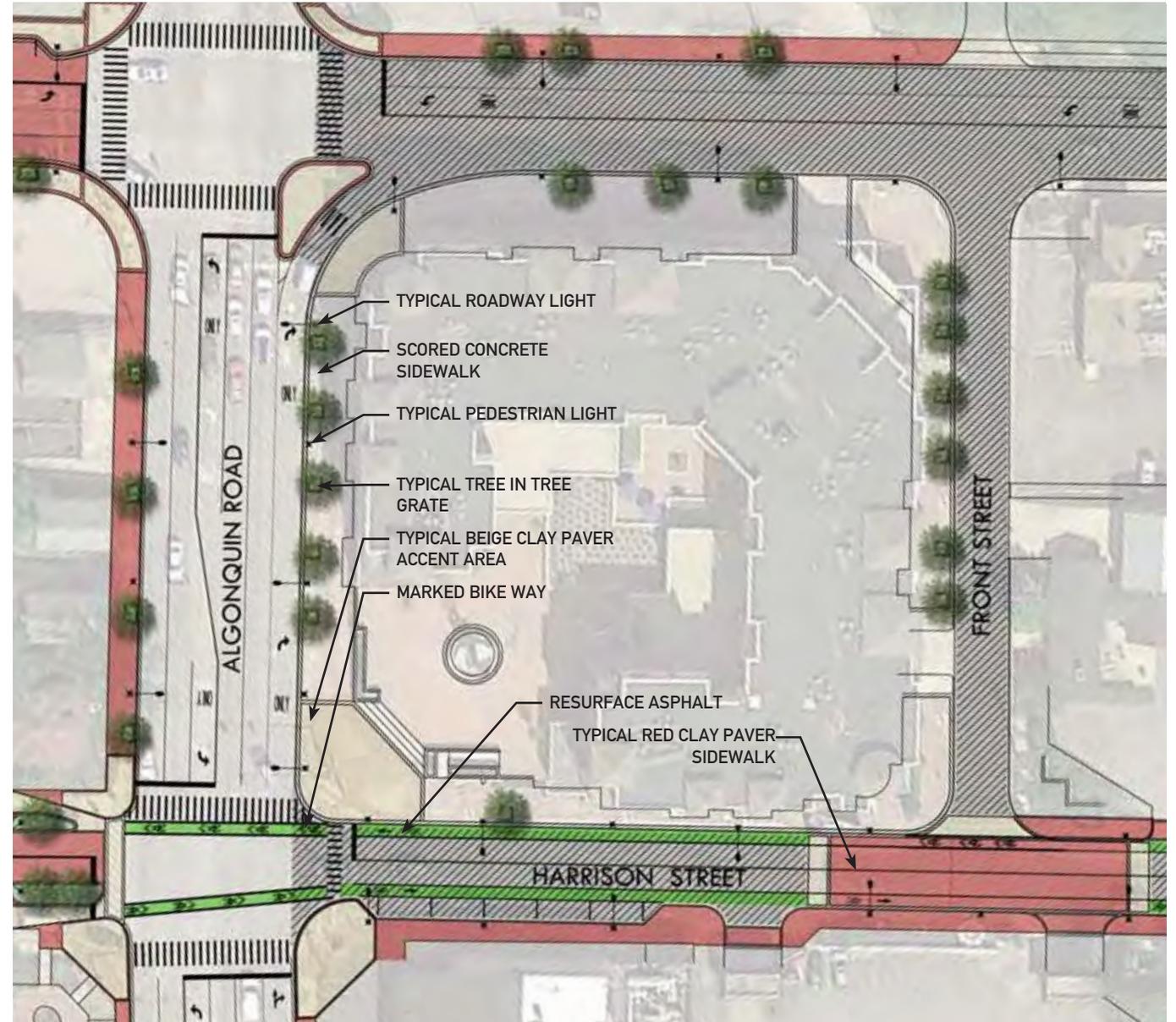
In general, the secondary streetscape palette should be followed. The intersection of Harrison and Front Street currently has red clay pavers for surfacing, which should be maintained. However, the crosswalks should use a contrasting material, which is recommended to be the beige clay pavers.

The south side of Algonquin Road currently has a narrow sidewalk. However, the Village controls many of the sites and is actively pursuing redevelopment. If possible, the sidewalk should be widened to increase pedestrian comfort and allow for the inclusion of street trees.

LEGEND

	Red Clay Pavers		Landscape Enhancement
	Beige Clay Pavers		Grass Parkway
	Scored Concrete		Building
	Resurfaced Asphalt		NORTH
	Reconstructed Asphalt		
	Marked Bike Way		

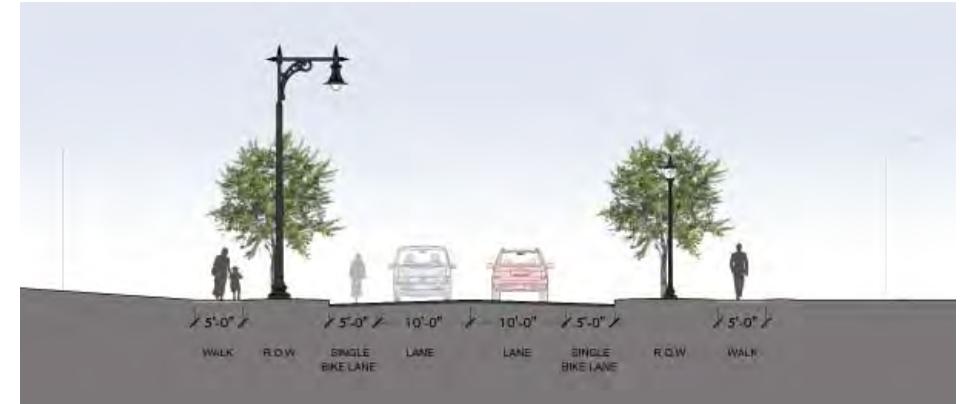
CONCEPTUAL STREETScape MASTER PLAN - RIVERSIDE PLAZA BLOCK



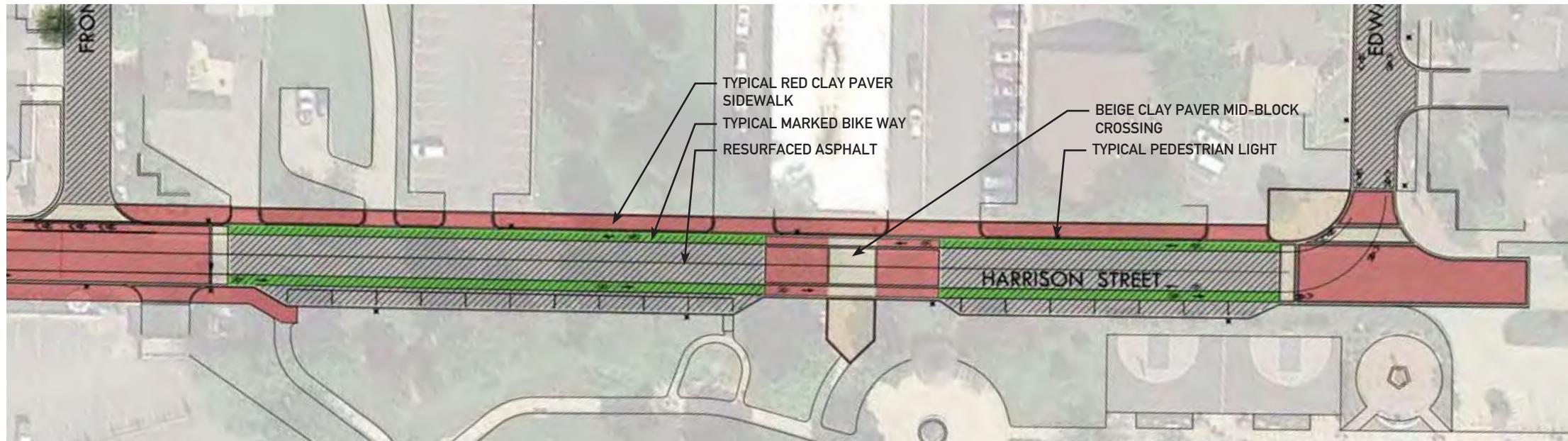
Harrison Street - Front to Edwards

The continuation of Harrison Street to the northeast runs adjacent to Riverfront Park, where there is no immediate sidewalk and streetscape. The sidewalk on the opposite side from the park is currently red clay pavers, with a narrow grass parkway. The plan shows the geometry of Harrison modified to incorporate bike lanes, which eliminates space for a parkway. There is not sufficient room for street trees, however this area has several trees immediately outside of the right-of-way that could be preserved.

The plan shows the existing mid-block crossing to the park upgraded with a central section of beige clay pavers to create improved contrast and visibility.



CONCEPTUAL CROSS-SECTION - HARRISON STREET BETWEEN FRONT ST AND EDWARDS ST



CONCEPTUAL STREETScape MASTER PLAN - HARRISON STREET FROM FRONT STREET TO EDWARDS STREET

Tertiary Streets

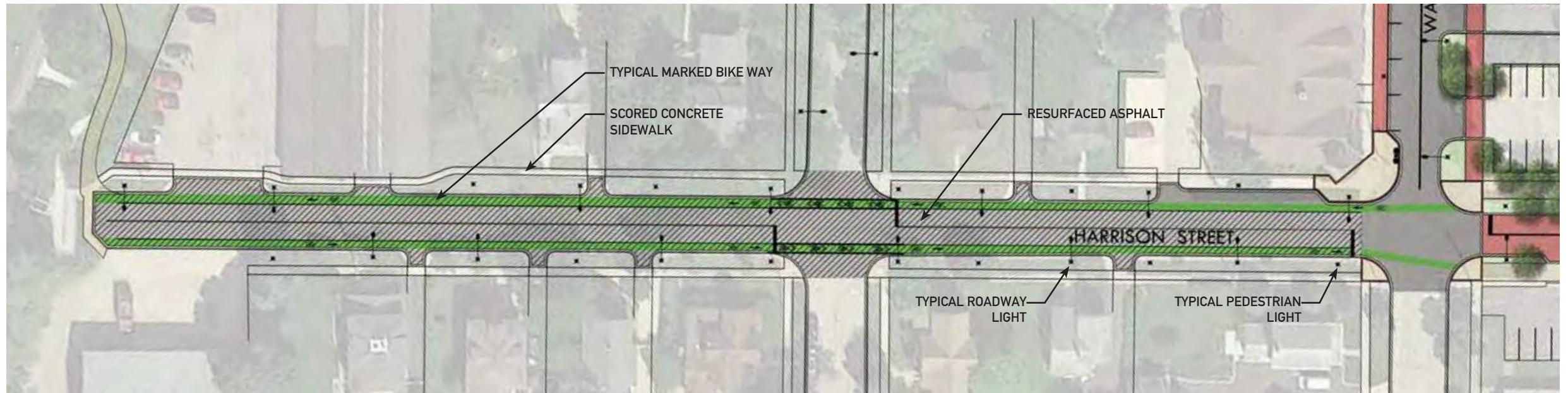
Harrison Street - Prairie Trail to Washington

This segment of Harrison is important for the bicycle connection to the Prairie Trail, however, due to the residential character, it is considered a tertiary street.

The plan shows resurfacing of the street and re-striping to create on-street bike lanes. Concrete sidewalks should be installed where existing sidewalks are failing or in poor condition. Additionally, a mix decorative overhead roadway lights and pedestrian lights should be incorporated into this area. A drop-off lane should be considered in front of the Congregational Church.



CONCEPTUAL CROSS-SECTION - HARRISON STREET BETWEEN PRAIRIE TRAIL AND WASHINGTON ST



CONCEPTUAL STREETScape MASTER PLAN - HARRISON STREET FROM PRAIRIE TRAIL TO WASHINGTON STREET

Main Street - Front to Edwards

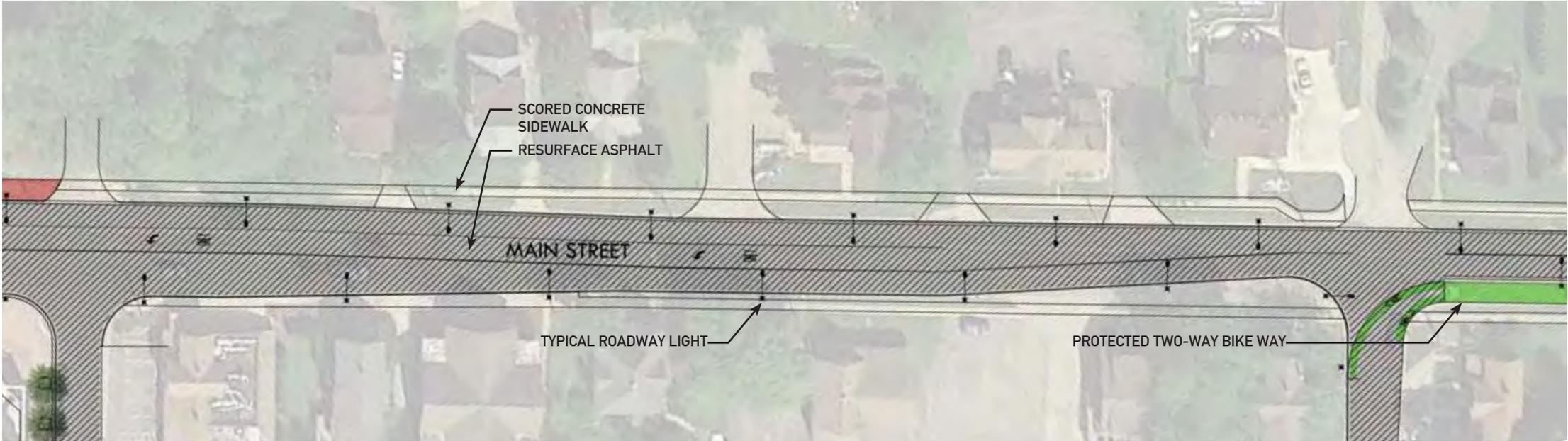
This segment of Main creates a transition from the active primary segments of Main to the section to the northeast, that the plan shows modified to incorporate bicycle lanes.

The geometry is not changed dramatically in this section, though it does taper from a three lane cross-section to a two lane cross-section at Edwards.

There are limited streetscape enhancements recommended beyond decorative overhead lights. It should be noted that if the adjacent properties were to redevelop and incorporate more intense commercial uses, this area should be upgraded to a primary street.

LEGEND

	Red Clay Pavers		Landscape Enhancement
	Beige Clay Pavers		Grass Parkway
	Scored Concrete		Building
	Resurfaced Asphalt		NORTH
	Reconstructed Asphalt		
	Marked Bike Way		



CONCEPTUAL STREETScape MASTER PLAN - MAIN STREET FROM FRONT STREET TO EDWARDS STREET

Main Street - North of Edwards

The design of Main Street north of Edwards changes more dramatically. The goals of this area are to slow down traffic significantly, and incorporate better bicycle and pedestrian connections.

The roadway is narrowed to two lanes to provide traffic calming and create additional space for bicycle facilities.

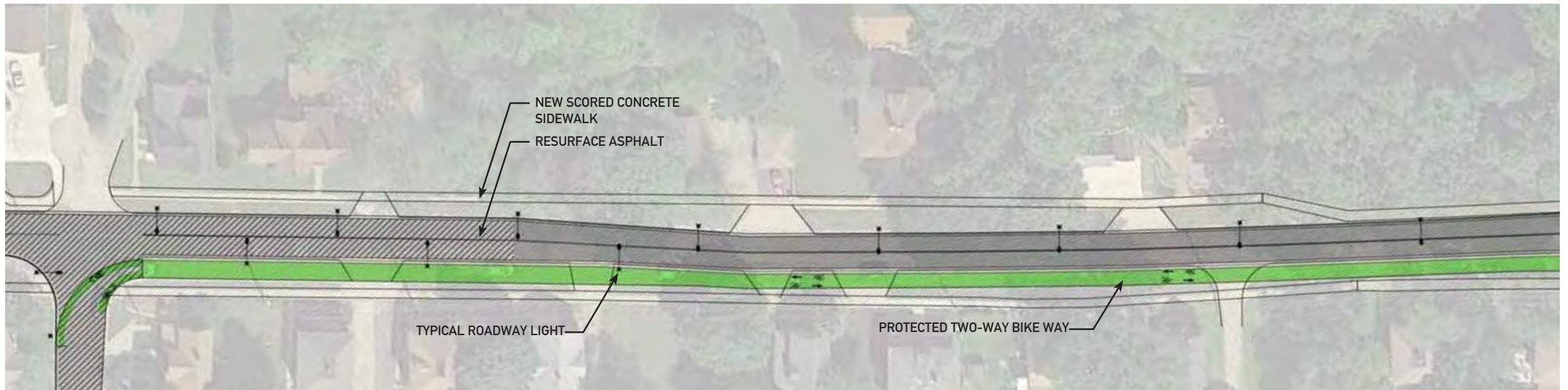
The plan recommends a consistent sidewalk on the northwest side of the road, connecting the neighborhood to the northwest to the downtown.

Additionally, the plan calls for a protected, two-way set of bike lanes on the southeast side.

This area should receive decorative overhead roadway lights as part of the enhancements.

Additionally, it should be noted that the plan shows Riverside Drive between Main Street and Park Street closed. Currently this only allows northbound Main Street traffic to enter into the neighborhood and access Park Street. The closure will allow for increased continuity and safety for the bike lanes, and will have minimal impact on access to the neighborhood, requiring northbound cars to turn one block earlier at Edwards.

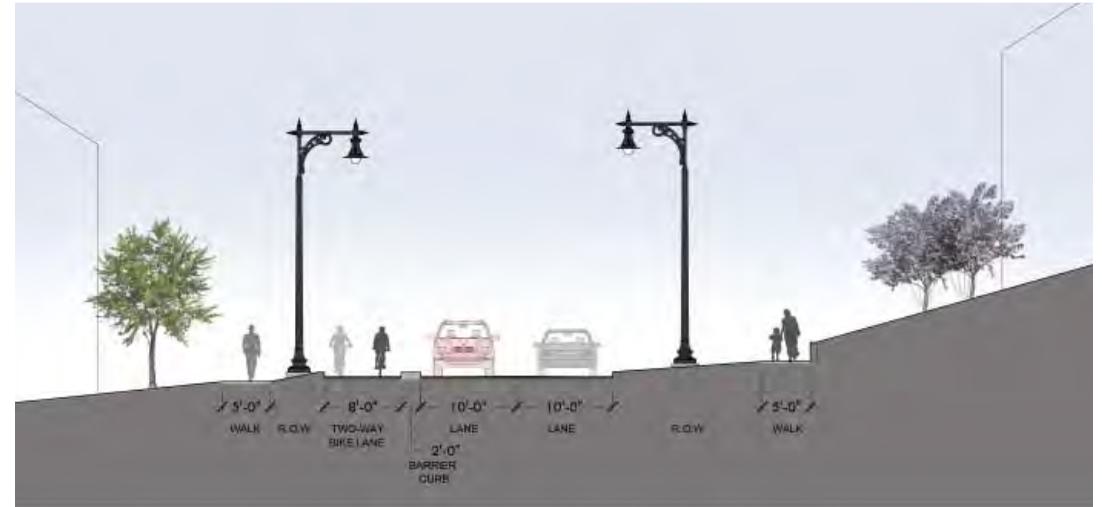
The plan recommends an optional solution approach to the intersection of Main and Arrowhead Drive / Cary Road. As shown on page 28, this intersection could be reconfigured with a roundabout, which would help slow traffic and address issues with turning movements at this intersection related to traffic speeds and visibility. The roundabout was designed and tested to ensure that it conceptually works and the Village will need to ultimately decide if it is the preferred direction.



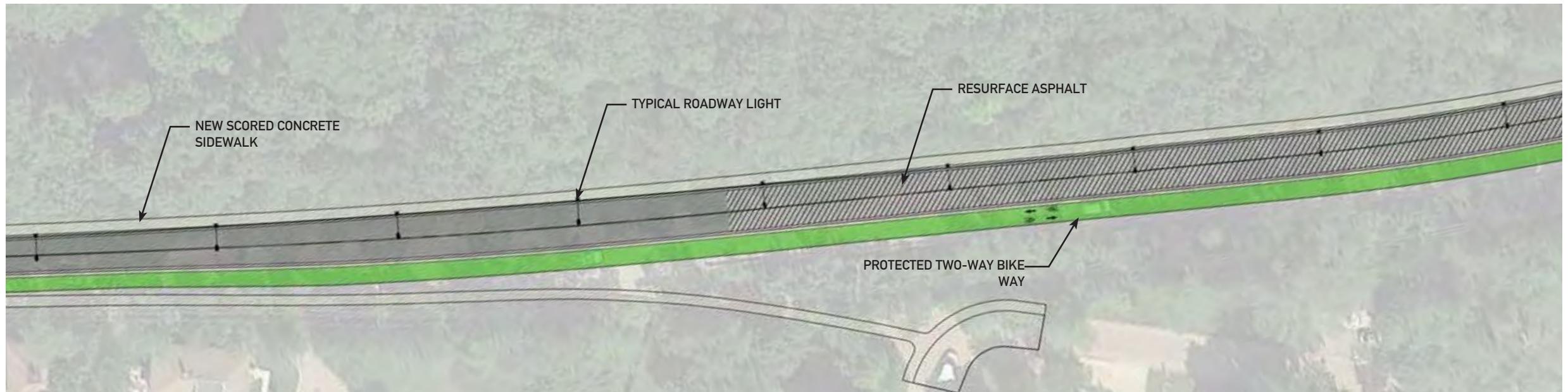
CONCEPTUAL STREETScape MASTER PLAN - MAIN STREET EXTENDING NORTH FROM EDWARDS STREET

LEGEND

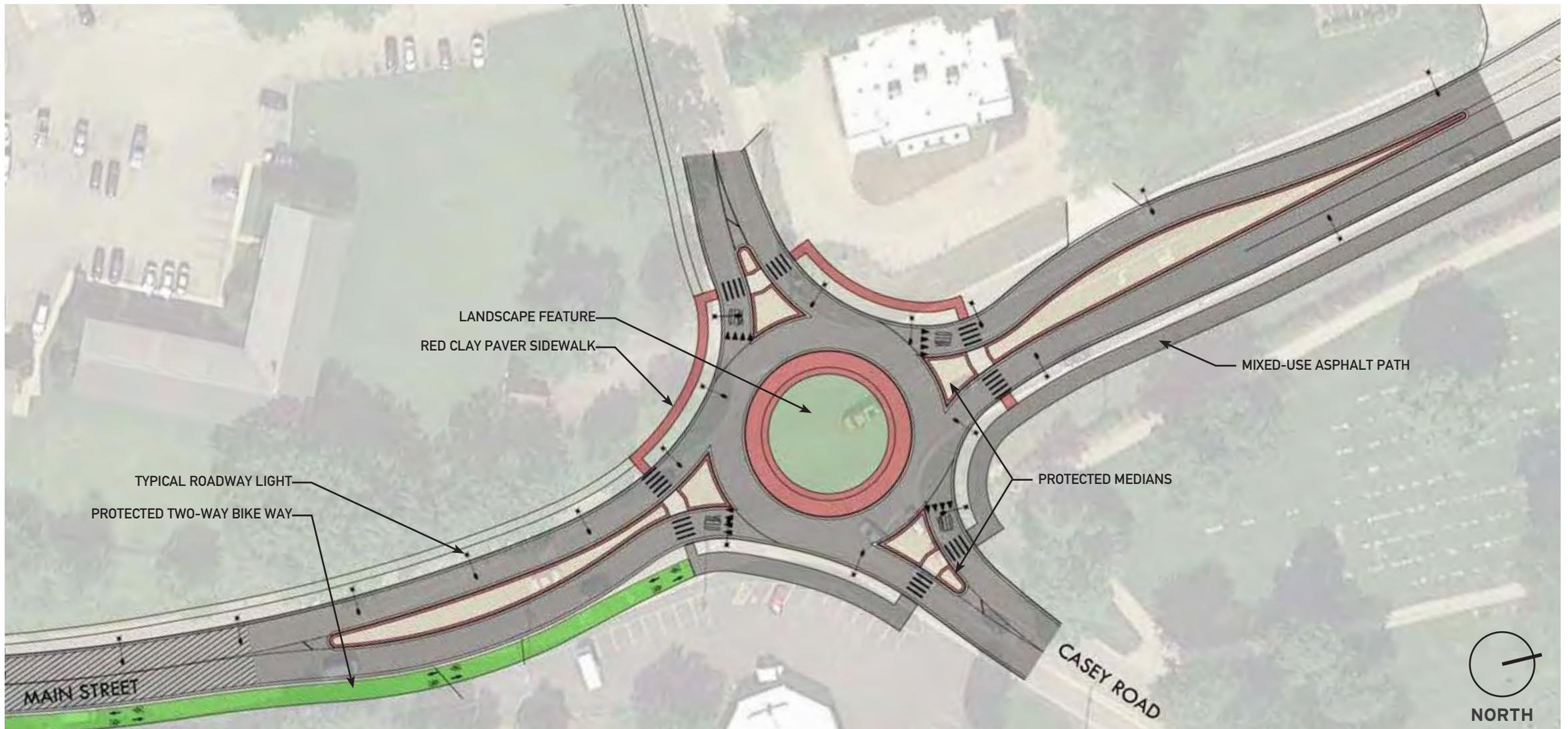
- | | | | |
|---|-----------------------|---|-----------------------|
|  | Red Clay Pavers |  | Landscape Enhancement |
|  | Beige Clay Pavers |  | Grass Parkway |
|  | Scored Concrete |  | Building |
|  | Resurfaced Asphalt |  | NORTH |
|  | Reconstructed Asphalt | | |
|  | Marked Bike Way | | |



CONCEPTUAL CROSS-SECTION - MAIN STREET - NORTH OF EDWARD STREET



CONCEPTUAL STREETScape MASTER PLAN - MAIN STREET NORTH OF EDWARDS STREET (ADJACENT TO PARK STREET)

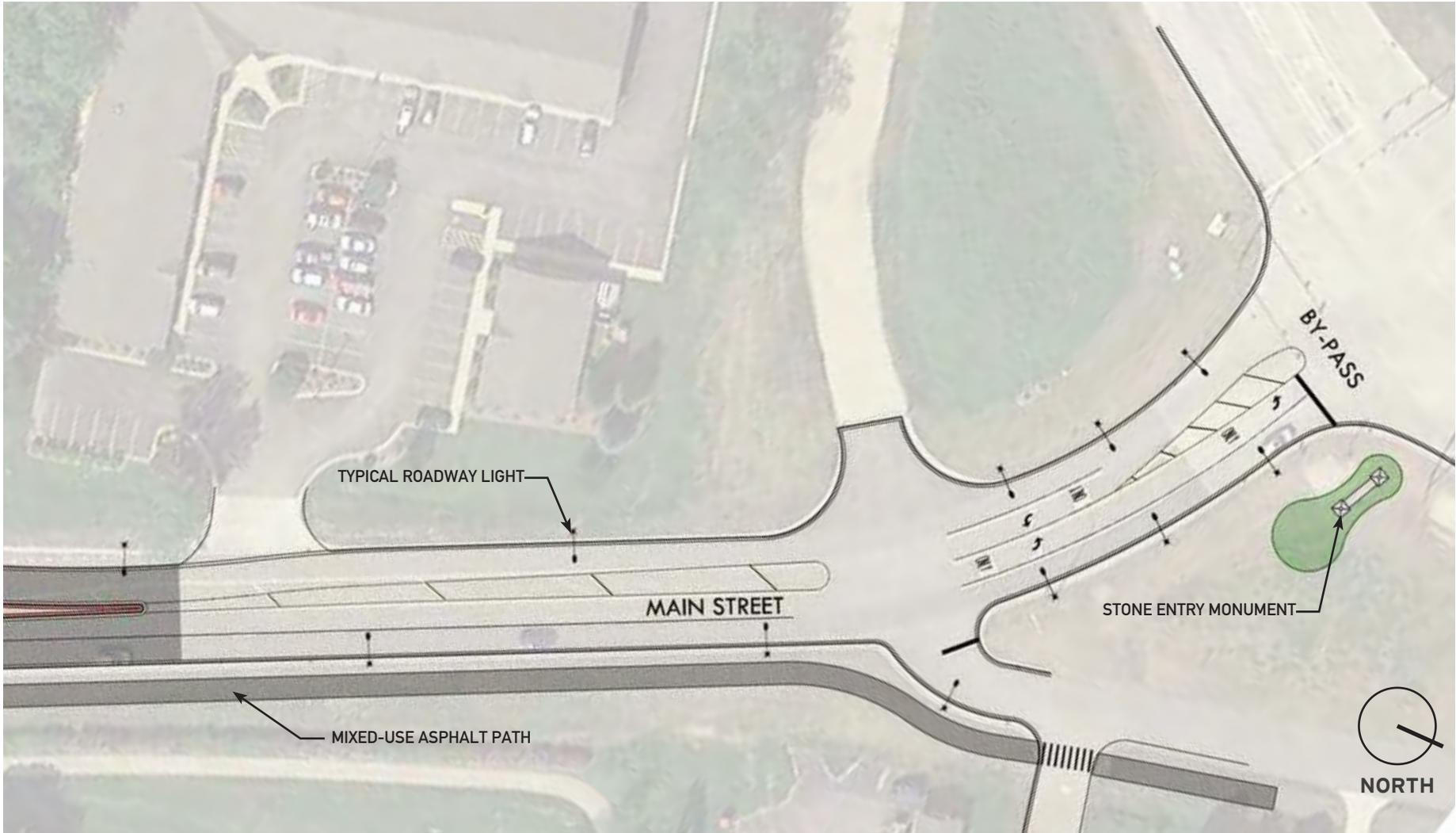


CONCEPTUAL STREETScape MASTER PLAN - OPTIONAL ROUNDABOUT CONCEPT OF MAIN STREET, CARY ROAD AND ARROWHEAD DRIVE

Main Street - North By-pass Entry

The northern entrance to Main Street off of the Illinois Route 31 By-pass provides another opportunity to create a gateway element and establish identity for the downtown. Site lines and visibility for motorists will be critical in this location, but there is sufficient room to implement a gateway monument and supplemental landscaping.

The plan also shows the bike lanes along Main Street connecting into the regional trail along IL Route 31.



LEGEND

	Red Clay Pavers		Landscape Enhancement
	Beige Clay Pavers		Grass Parkway
	Scored Concrete		Building
	Resurfaced Asphalt		
	Reconstructed Asphalt		
	Marked Bike Way		

CONCEPTUAL STREETScape MASTER PLAN - MAIN STREET AT ILLINOIS ROUTE 31 BY-PASS

Specialty Features

Overall

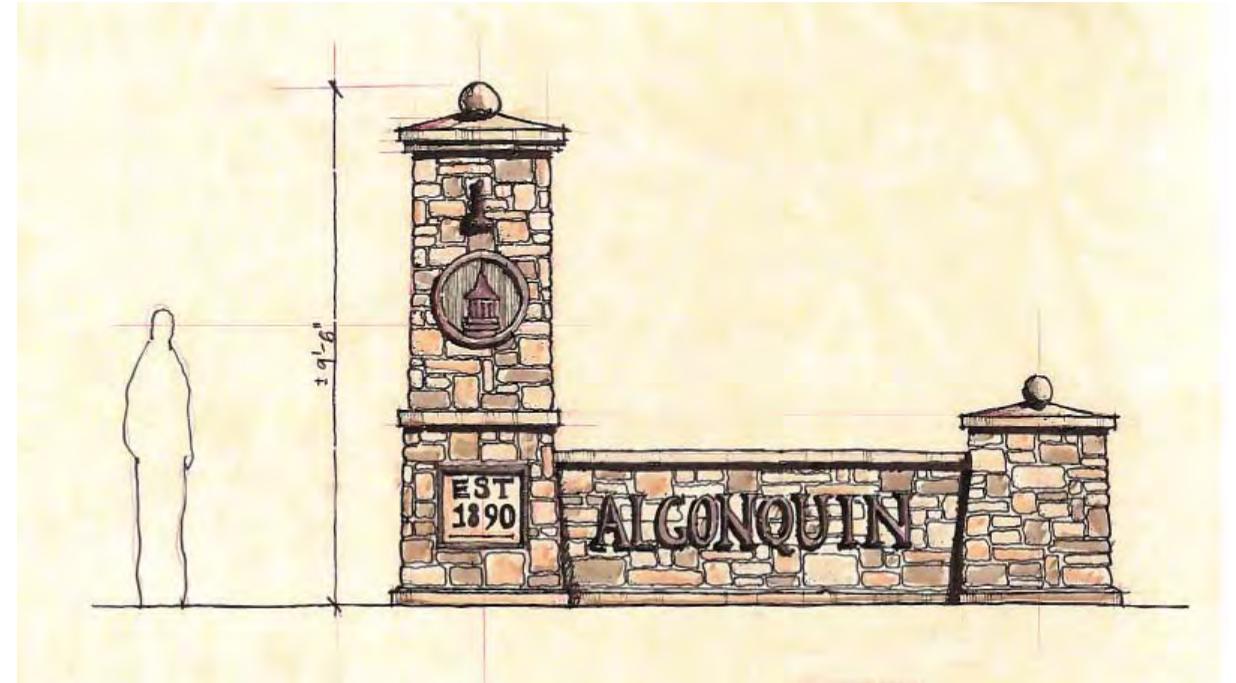
During the design process, concepts were developed for a range of specialty features for the downtown. These elements should tie into local character, architecture and history to help enhance downtown's sense of place and authenticity.

The images on the following pages are concepts only, created to convey the idea, test interest with the community, and build excitement for the type of character the streetscape could establish.

If supported, these features will need to be more fully designed as part of future phases of work and more detailed estimates of cost will need to be developed.

Gateway Entry Features

The plan recommends gateway entry features in several locations in the downtown. These elements should relate to other stone walls and piers currently found in the downtown parks and bridges. The walls can be enhanced with metal pin-set lettering and a cast metal medallion to reinforce the Village identity.



STONE GATEWAY ENTRY FEATURE CONCEPT

Main Street Bridge

The bridge on Main Street over Crystal Creek is a unique opportunity for placemaking in the downtown. Few downtowns in the Chicago region have bridges to proximate to the commercial main street. As part of the overall streetscape, the bridge should be used to narrow the roadway and increase the amount of pedestrian space. The bridge should create a comfortable environment for people to stop and linger.

The concept shows a decorative pergola over the pedestrian walks, with a fence and railing separating these areas from the roadway. Stone columns and an archway could be used to highlight this entrance onto Main Street. Enhanced lighting and planters for annual flowers could further improve the character of this space.



EXISTING CONDITIONS



MAIN STREET BRIDGE CONCEPT

Decorative Railing

The plan recommends a decorative railing be used that has connections to the historic character of the downtown. As shown in the picture to the right, the historic Village Hall at the corner of Main Street and Algonquin Road originally had an outdoor stairway on its west side that include and metal railing. This design pattern could be used throughout the downtown in various locations.

One potential use would be to unify the commercial area of downtown by placing this railing along edges of raised entrances to storefronts. These raised storefronts create a challenge in the downtown from an accessibility standpoint and due to the amount of sidewalk they take up. By enhancing these elements with a decorative railing, and potentially through integrated benches and planters, they could be more attractive and reinforce and character and identity throughout the main street shopping area.



HISTORIC PHOTO OF RAILING



DOWNTOWN DECORATIVE RAILING CONCEPT

Directional Signage

While the downtown is not immense, it would benefit from increased wayfinding to help direct new visitors and shoppers to parking and other key downtown assets.

Directional signs should be installed on existing light poles if possible, and reinforce the Village's brand through use of colors and fonts that are consistent with the Village logo and branding.



WAYFINDING SIGNAGE CONCEPT

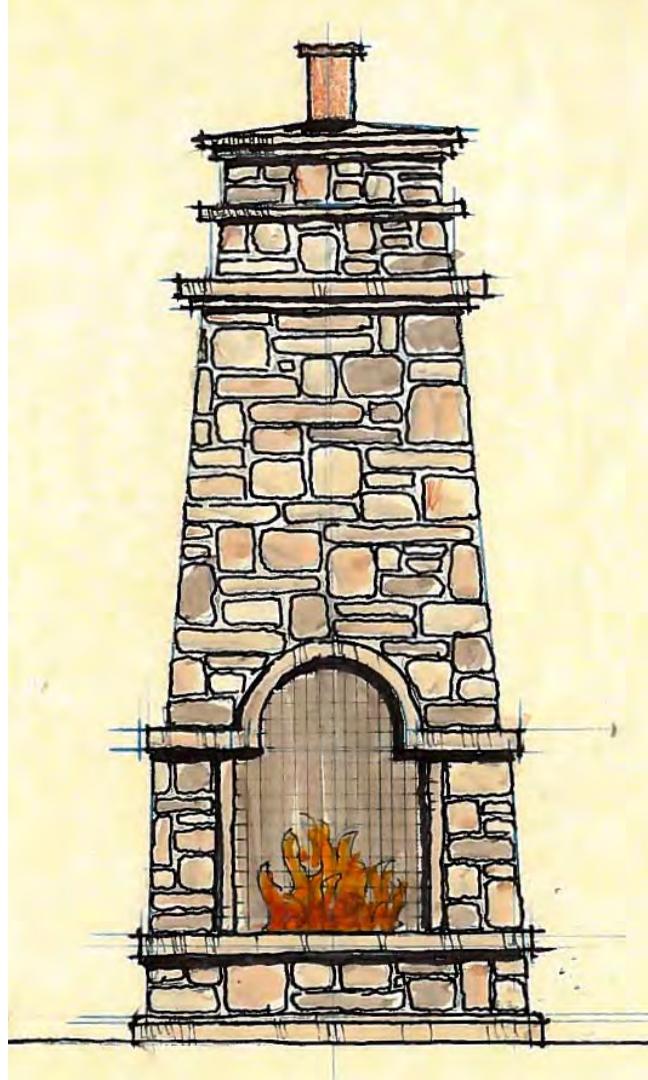
Main Street Plaza

The existing plaza at the northwest corner of Main Street and Algonquin Road could be renovated and enhanced to better serve the community. The current design provides landscape, a flag court, a community events sign, and had been the location of a fountain feature. Currently, there is no room for seating and it does not invite people into the space, rather provides something for people to look at as they pass by.

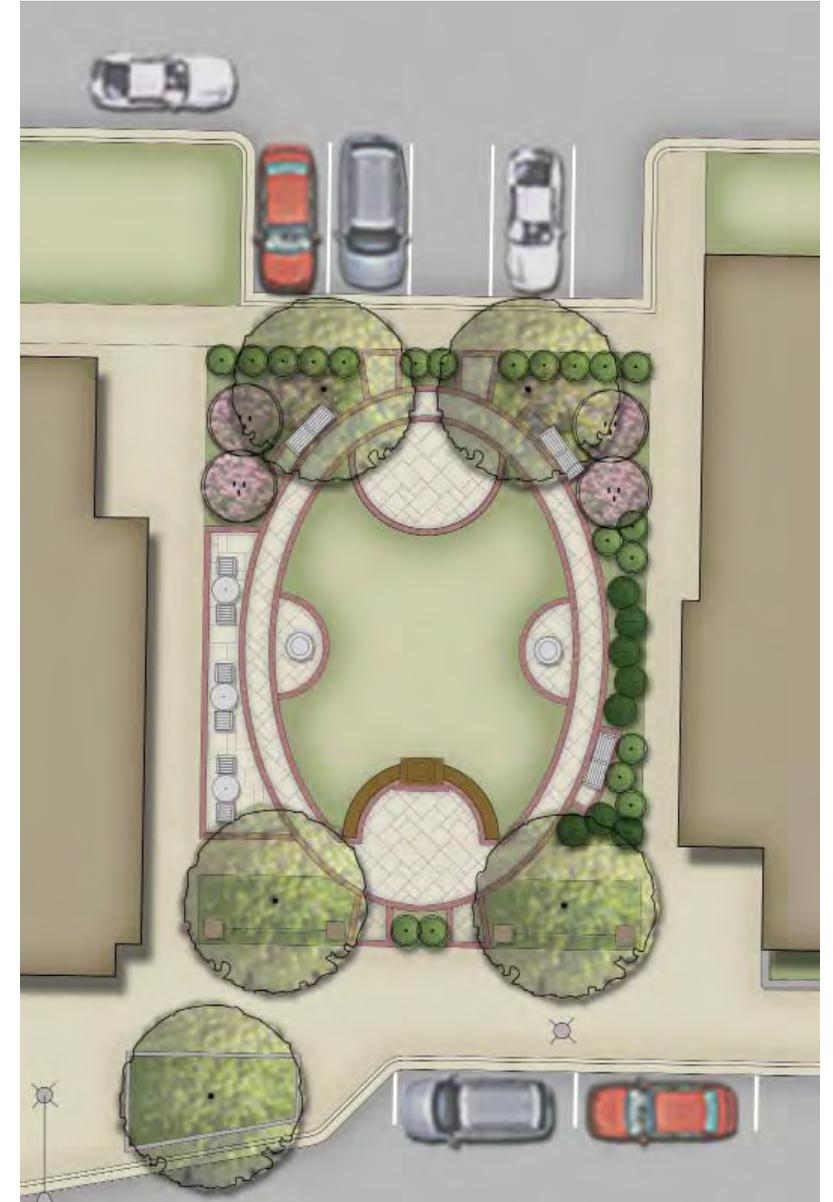
The concept shows a more open space with a central grass area and landscape at the perimeter. Additional public seating could be integrated, as well as room for additional outdoor dining to support the adjacent restaurant.

To help create a “four seasons” downtown, the plaza could include a fire place that would make the downtown more appealing in the winter.

Other elements could easily be integrated into the final design, including a fountain or a new community events sign that relates both to pedestrians and vehicles.



FIRE PLACE CONCEPT



DOWNTOWN PLAZA CONCEPT

Alley Enhancements

There are numerous pedestrian alleys along Main Street that connect to rear accessible entrances or parking areas. These alleys could be enhanced in a way that complements the streetscape and offers other opportunities. They could include additional landscape through movable planters or hanging baskets, decorative lighting, seating areas and public art.

Given the relationship of these spaces to private sites, any improvements would have to be closely coordinated between the Village and private owners. Depending on whether the alley is a public right-of-way or not, the land owner may be solely responsible for any improvements. However, the Village could choose to provide design and engineering services at the same time as streetscape work is being completed to provide economies of scale. The Village could also chose to go further to help with funding and implementation.



PEDESTRIAN ALLEY CONCEPT

EXISTING CONDITIONS

Crystal Creek Enhancements

The Plan recommends improvements and enhancements to Crystal Creek and the adjacent land to the northeast to create a unique feature creating pedestrian linkages that connect Cornish Park and Harrison Street to Main Street, and further up to Towne Park.

This concept assumes that much of the site border by Main Street, Algonquin Road, and Harrison Street will be redeveloped. However, the concept

shows Creekside Tap preserved, with their parking relocated to the southeast.

The concept seeks to improve and restore the banks of the creek to stabilize them and improve the aesthetics of the creek edge. Additionally, the hydrology of the creek should be further assessed to identify any opportunities for improvement, that would slow and filter the water and reduce erosion.

A trail is shown on the northeast side of the creek along with landscape pockets and outdoor seating areas. The plan could accommodate outdoor dining or patio space for Creekside Tap to enhance the site and create activity along the creek walk.

In addition to these improvements, aesthetic and functional improvements to the bridges should be addressed where feasible, which may include modifications to utilities that are connected to the bridges.



CRYSTAL CREEK ENHANCEMENT CONCEPT



Downtown Parking Enhancements

The Plan also recommends further study of creating a shared public lot within the area bounded by Main Street, Washington Street, and Harrison Street.

This area has several individual private parking lots. Due to their size and shape, it results in inefficient parking, poor circulation and numerous curb cuts onto Harrison Street.

The proposed enhancements could increase the number of spaces by about 5%, reduce curb-cuts on Harrison by 50%, introduce trees and landscaped islands into the parking lot, and improve circulation. The plan also allows for a mid block pedestrian connection that would link a pedestrian alley from Main Street down to Harrison Street, improving pedestrian circulation.

Clearly, to further this concept, the Village would have to work with private property owners and look for solutions that may include developing cross-access easements and other agreements.



DOWNTOWN PARKING ENHANCEMENT CONCEPT



Mineral Springs

The plan also suggests enhancements to the historic mineral springs site near Towne Park, northwest of Main Street.

This feature includes a ring of concrete steps around a location that used to produce spring water. The spring location is now capped, The site does not receive much attention as it is in disrepair, is not very visible and is not directly connected to any pedestrian trails. However, both the Historic Commission and the Garden Club have expressed interest in renovating it.

This concept suggests replacing the concrete steps with a weathered natural stone. In the center, where the spring was located, the concept shows a metal medallion that could have historic information about the site. When the spring was active, the water would flow into the adjacent Crystal Creek, and this is re-imagined through a swath of plantings that would extend from the spring to the creek.

The final plan should be developed in collaboration with the Historic Commission and Garden Club to ensure it meets community goals.



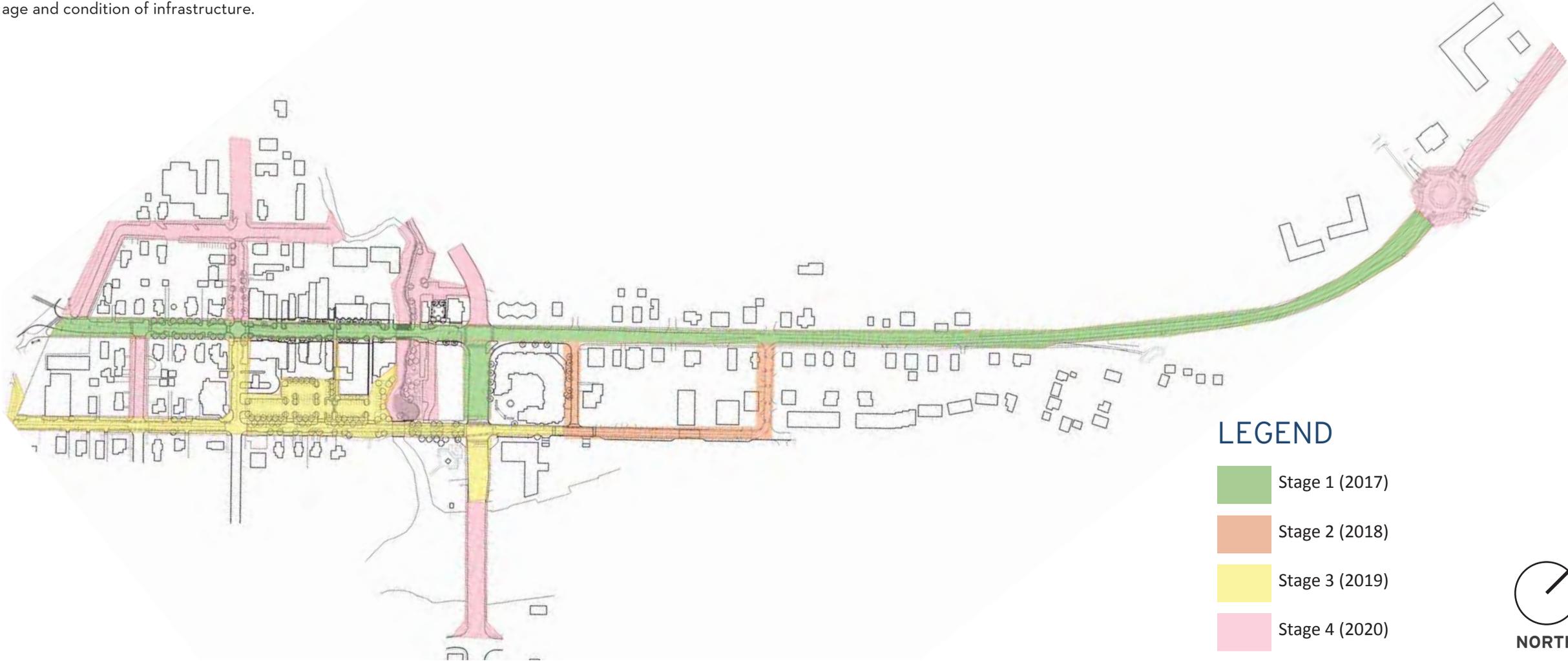
MINERAL SPRINGS ENHANCEMENT CONCEPT



EXISTING CONDITIONS

Project Staging

The diagram on this page presents the conceptual phasing or staging of improvements based on community priorities, potential funding sources, and age and condition of infrastructure.



Preliminary Costs

Based upon the streetscape master plan, as well as the understanding of age and condition of utilities that should be addressed as part of streetscape improvements, the following preliminary estimates of cost have been prepared.

Cost will be refined and further detailed as plans are further detailed in future phases of work.

	ROAD/STREETScape	BRIDGES	TOTAL
MAIN STREET			
• BYPASS TO ALGONQUIN	\$5.0 M	\$1.8 M	\$6.8 M
• ALGONQUIN TO EDWARD	\$0.8 M		\$0.8 M
• EDWARD TO BYPASS (W/ROUNDABOUT)	\$4.4 M		\$4.4 M
HARRISON STREET	\$4.5 M	\$1.2 M	\$5.7 M
WASHINGTON STREET	\$1.3 M	\$1.3 M	
ALGONQUIN ROAD	\$1.2 M	\$2.4 M	\$3.6 M
<hr/>			
JEFFERSON STREET			
RAILROAD ROAD			
MADISON STREET	\$0.6 M		\$0.6 M
FRONT STREET			
EDWARD STREET			
	<hr/>	<hr/>	
	\$17.8 M	\$5.4 M	
<hr/>			
HARRISON PARKING LOTS			\$0.5 M
CRYSTAL CREEK			\$1.3 M
			<hr/>
			\$25.0 M
		A/E	\$5.0 M
		TOTAL	\$30.0 M

AVERAGE COST RANGES

Across the entire study area, the average cost of improvements is approximately \$1,500 per linear foot. The following are cost ranges based upon the streetscape hierarchy:

- Primary: \$2,000 - \$3,000/ft.
- Secondary: \$1,000 - \$2,000/ft.
- Tertiary: \$300 - \$1,000/ft.