

AGENDA
COMMITTEE OF THE WHOLE
July 24, 2018
2200 Harnish Drive
Village Board Room
- AGENDA -
7:30 P.M.

Trustee Sosine – Chairperson
Trustee Spella
Trustee Jasper
Trustee Brehmer
Trustee Glogowski
Trustee Steigert
President Schmitt

1. **Roll Call – Establish Quorum**
2. **The Village of Algonquin Proclaims August 7, 2018 as National Night Out**
3. **Public Comment – Audience Participation** (*Persons wishing to address the Committee on an item on this agenda must register with the Chair prior to roll call.*)
4. **General Administration**
 - A. Consider an Ordinance Amending Chapter 33, Liquor Control and Liquor Licensing, Amending the Number of Available A-3 Liquor Licenses by One
5. **Community Development**
 - A. Consider a Special Event Permit and Special Liquor License for St. Margaret Mary's Summer Fest 2018 to be Held August 26, 2018
 - B. Consider a Special Event Permit for Algonquin Aces Fall Fling Softball Tournament to be Held September 29 and 30, 2018
 - C. Consider a Special Event Permit for the Jacobs High School Homecoming Parade to be Held September 23, 2018
 - D. Consider a Request for Re-Zoning, Final Plat, Final PUD and Special Use Permit for 2401 Huntington Drive North Case No. 2018-09. CarMax Auto Superstore
 - E. Consider the CarMax Business Development Agreement
6. **Public Works & Safety**
7. **Executive Session**
8. **Other Business**
9. **Adjournment**

PROCLAMATION NATIONAL NIGHT OUT 2018

WHEREAS, the National Association of Town Watch (NATW) is sponsoring a unique, nationwide crime, drug and violence prevention program on August 7th, 2018 entitled "National Night Out"; and

WHEREAS, the "35th Annual National Night Out" provides a unique opportunity for the Village of Algonquin to join forces with thousands of other communities across the country in promoting cooperative, police-community crime prevention efforts; and

WHEREAS, the Village of Algonquin plays a vital role in assisting the Algonquin Police Department through joint crime, drug and violence prevention efforts in Algonquin and is supporting "National Night Out 2018" locally; and

WHEREAS, it is essential that all citizens of Algonquin be aware of the importance of crime prevention programs and impact that their participation can have on reducing crime, drugs and violence in the Village of Algonquin; and

WHEREAS, police-community partnerships, neighborhood safety, awareness and cooperation are important themes of the "National Night Out" program;

NOW, THEREFORE I, John Schmitt, Village President, do hereby call upon all citizens of the Village of Algonquin to join the Algonquin Police Department and the National Association of Town Watch in supporting "35th Annual National Night Out" on August 7th, 2018.

FURTHER, LET IT BE RESOLVED THAT, I, John Schmitt, Village President, do hereby proclaim Tuesday, August 7th, 2018 as "NATIONAL NIGHT OUT" in the Village of Algonquin.

IN WITNESS THEREOF, I have set my hand and seal this 24th day of July, Two Thousand and Eighteen, A.D.

John C. Schmitt, Village President

(SEAL)

Attest:

Gerald S. Kautz, Village Clerk



VILLAGE OF ALGONQUIN
MEMORANDUM

DATE: July 18, 2018

TO: Committee of the Whole

FROM: Michelle Weber

SUBJECT: Liquor Code Amendment

In accordance with an ordinance passed in 2013 limiting the number of allowable liquor licenses in all classes to the number of licenses issued at that time, the attached proposed ordinance increases the number of available Class A-3 liquor licenses by one. This change is the result of requests from:

- Cattleman's Burger & Brew, Inc., 205 S. Main Street, Algonquin, a new restaurant opening soon in our downtown area. This license will allow them to serve alcohol for consumption on premises, and shall permit the sale of wine in sealed containers for consumption off premises.

Staff recommends that the change in the number of available licenses be approved.

Attachment

ORDINANCE NO. 2018-O-XX

An Ordinance Amending Chapter 33, Liquor Control and Liquor Licensing, of the Algonquin Municipal Code

WHEREAS, the Village of Algonquin, McHenry and Kane Counties, Illinois, is a home rule municipality as contemplated under Article VII, Section 6, of the Constitution of the State of Illinois, and the passage of this Ordinance constitutes an exercise of the Village's home rule powers and functions as granted in the Constitution of the State of Illinois.

NOW, THEREFORE, BE IT ORDAINED by the President and Board of Trustees of the VILLAGE OF ALGONQUIN, McHenry and Kane Counties, Illinois, as follows:

SECTION 1: Section 33.07-B, Paragraph 4 Number of Licenses Issued, of the Algonquin Municipal Code shall be amended as follows:

4. Seven Class A-3 licenses at one time.

SECTION 2: If any section, paragraph, subdivision, clause, sentence or provision of this Ordinance shall be adjudged by any Court of competent jurisdiction to be invalid, such judgment shall not affect, impair, invalidate or nullify the remainder thereof, which remainder shall remain and continue in full force and effect.

SECTION 3: All ordinances or parts of ordinances in conflict herewith are hereby repealed to the extent of such conflict.

SECTION 4: This Ordinance shall be in full force and effect August 7, 2018, approval and publication in pamphlet form (which publication is hereby authorized) as provided by law.

Voting Aye:

Voting Nay:

Abstain:

Absent:

APPROVED:

(SEAL)

Village President John C. Schmitt

ATTEST: _____
Village Clerk Gerald S. Kautz

Passed:

Approved:

Published:



VILLAGE OF ALGONQUIN
COMMUNITY DEVELOPMENT DEPARTMENT

– M E M O R A N D U M –

DATE: July 20, 2018
TO: Committee of the Whole
FROM: Russell Farnum, AICP, Community Development Director
SUBJECT: *Upcoming Special Event Requests*

Attached please find applications for the following proposed special events:

1. St. Margaret Mary Summerfest 2018, August 26 (including a liquor license);
2. Jacobs High School Homecoming Parade, September 23;
3. Algonquin Aces Fall Fling, Softball Tournament, September 29 and 30.

These events will require Board approval due to either use of public property or the necessity of having a liquor license.

All three applications have been reviewed and are in good order, the only issue is the permission of Holiday Inn and Colonial Café for their accesses to be completely blocked during the time of the Homecoming Parade.

All of the petitioners have been notified of the Committee meeting and should be present to answer any questions or address any concerns.



RECEIVED

JUL 10 2018

COMMUNITY
DEVELOPMENT

July 2, 2018

Village of Algonquin
2200 Harnish Dr.
Algonquin, IL. 60102

It is time to begin preparation for our Saint Margaret Mary Summerfest 2018.

We plan to have a Beer Tent, Food Booths, and Live Entertainment.

"Saint Margaret Mary Summerfest 2018" will be a one-day event. This is a family orientated event; we strive to have appropriate games and entertainment for all ages. The day begins at 12:00 Noon. We will end the event at 7:00 pm so not to cause disruptions to our neighbors.

We have controls in place to insure NO underage drinking, wrist bands will be given to persons 21 and over with the proper identification. We are providing security with volunteers from our parish. I have attached a list of their names with this application and a copy of this application is being forwarded to the Chief of Police for his review.

I have submitted an application for an Event Liquor Permit.

If you have any questions please feel free to call me at 847 658-7625.

Sincerely,

Linda M. Settles, Office Manager
Saint Margaret Mary Church

cc: Chief of Police



Village of Algonquin
PUBLIC EVENT/ENTERTAINMENT LICENSE APPLICATION

In order for the Village of Algonquin to assist you with your Public Event, please fill out the information below and return to Diane LaCalamita at the Ganek Municipal Center (2200 Harnish Drive or dlacalamita@algonquin.org) at least 45 days prior to the event.

Please type or print legibly.

Official Name of the Event: Saint Margaret Mary Summerfest 2018

Sponsoring Organization:

Name: Saint Margaret Mary Church Contact Name: Margaret Przybylko
Address: 111 S. Hubbard Street
City, State, ZIP: Algonquin, Illinois 60102
Phone: 847-858-7625 Email: mprzybylko@saintmargaremary.org

Event Coordinator:

Name: Monika Marat
Home Address: [REDACTED]
City, State, ZIP: [REDACTED]
Phone: [REDACTED] Email: [REDACTED]

Event Information:

Describe the Nature of the Event: Community building and fellowship

New Event ☐ Repeat Event ☐ Yes ☐ No ☐ If repeat, will anything be different this year? ☐

Event Address: 119 S. Hubbard Street, Algonquin, Illinois 60102

Date(s) and Time(s) of the Event: August 26, 2018 12:00 PM - 7:00 PM

Rain Date(s), if applicable: No rain date

Set-Up Date/Time: Saturday, August 25, 2018 and Sunday, August 26, 2018 - 9:00 AM

Maximum Number of Attendees/Participants Expected: 700+

Admission Fee: Yes ☒ No ☐ If Yes, list fee(s) to be charged: \$5.00 Admission at tent

How will the revenue be used (include donations to non-profit or charitable organizations):

All revenue goes to Saint Margaret Mary Church

Event Website: www.saintmargaretmary.org

Event Details:

Describe provided security, including who will be providing the security (name and contact information), hours, and a security plan: _____

Parishioner volunteer roster attached to this application. Copy of this is forwarded to Chief of Police.

Describe parking or traffic control, including the location of extra parking and the number of spaces allocated, and how overflow parking will be handled: _____

Parish parking lot with additional handicapped spaces marked prior to event.

Will there be a need for road closures? Yes _____ No ^{No} If Yes, please explain: _____

Are you requesting Algonquin Police Officer(s) presence? Yes _____ No ^{No} If Yes, to perform what function? _____

Do you want a fire truck or ambulance present? Yes _____ No ^{No} If Yes, for what hours and to perform what function? _____

Are you wishing to post temporary sign(s) announcing the event? Yes ^{Yes} _____ No _____ If Yes, please describe desired size, location and date(s) that the signage will be displayed: _____

Permit will be requested through Parish Facility Manager

Do you wish to serve alcoholic beverages? Yes ^{Yes} _____ No _____

If Yes, do you have DRAM Shop Insurance for the sale/consumption of alcohol? Yes ^{Yes} _____ No _____ If Yes, attach a copy of the policy.

Will you have live entertainment? (e.g. bands, D.J., amplified sound, etc.) Yes ^{Yes} _____ No _____

If Yes, please describe type, band name(s), and hours of performance and if there will be a stage: _____

Various dance groups/and bands providing entertainment throughout the afternoon (12:00 PM) until the close of the Summerfest

In the evening (7:00 PM)

Do you foresee any other special needs for this event? (Physical set-up assistance, waste removal, portable toilets and hand washing stations, electricity, generator, running water, tent(s), etc.): _____

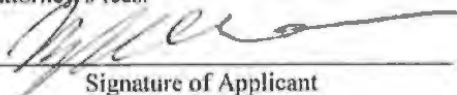
All items needed for the event are through the Facility Manager and Business Manger

Do you plan on holding a raffle during this event? Yes _____ No _____
(Must be an Algonquin-based, non-profit organization)

Name of on-site contact during the event (please print): _____ Monika Marat
On-site contact's cell number: _____
On-site contact's work number: _____
On-site contact's home number: _____

Affidavit of Applicant:

I, the undersigned applicant, or authorized agent of the above noted organization, swear or affirm that the matters stated in the foregoing application are true and correct upon my personal knowledge and information for the purpose of requesting the Village of Algonquin to issue the permit herein applied for, that I am qualified and eligible to obtain the permit applied for and agree to pay all fees, to meet all requirements of the Algonquin Village Code, and any additional regulations, conditions, or restrictions set forth in the permit and to comply with the laws of the Village of Algonquin, the State of Illinois, and the United States of America in the conduct of the Public Event described herein. In addition, Applicant certifies, by signing the application, that, pursuant to 720ILCS 5/11-9.4(c), no sex offenders are employed by the carnival operator, and that no carnival employees are fugitives from Illinois or any other state's law enforcement agencies. I (or the above named organization) further agree(s) to hold harmless and indemnify the Village, its officials, employees and successors and assigns, for any and all liability, damages, suits, claims and demands for damages at law or in equity it incurs as a result and arising either directly or indirectly out of the public event noted above including but not limited to damages and attorney's fees.


Signature of Applicant

6-27-2018
Date

MARGARET PRZYBYLKO
Printed Name of Applicant



CERTIFICATE OF LIABILITY INSURANCE

DATE (MM/DD/YYYY)
7/6/2018

THIS CERTIFICATE IS ISSUED AS A MATTER OF INFORMATION ONLY AND CONFERS NO RIGHTS UPON THE CERTIFICATE HOLDER. THIS CERTIFICATE DOES NOT AFFIRMATIVELY OR NEGATIVELY AMEND, EXTEND OR ALTER THE COVERAGE AFFORDED BY THE POLICIES BELOW. THIS CERTIFICATE OF INSURANCE DOES NOT CONSTITUTE A CONTRACT BETWEEN THE ISSUING INSURER(S), AUTHORIZED REPRESENTATIVE OR PRODUCER, AND THE CERTIFICATE HOLDER.

IMPORTANT: If the certificate holder is an ADDITIONAL INSURED, the policy(ies) must have ADDITIONAL INSURED provisions or be endorsed. If SUBROGATION IS WAIVED, subject to the terms and conditions of the policy, certain policies may require an endorsement. A statement on this certificate does not confer rights to the certificate holder in lieu of such endorsement(s).

PRODUCER Arthur J. Gallagher Risk Management Services, Inc. 2850 Golf Road Rolling Meadows IL 60008	CONTACT NAME: Gallagher Bassett Services, Inc.		
	PHONE (A/C, No, Ext): 414-203-4053	FAX (A/C, No): 414-258-1250	
INSURED Diocese of Rockford Finance & Administration Office P.O. Box 7044 Rockford IL 61125	INSURER(S) AFFORDING COVERAGE		NAIC #
	INSURER A: National Catholic RRG, Inc		10083
	INSURER B: Safety National Casualty Corporation		15105
	INSURER C:		
	INSURER D:		
	INSURER E:		
INSURER F:			

COVERAGES

CERTIFICATE NUMBER: 2146525497

REVISION NUMBER:

THIS IS TO CERTIFY THAT THE POLICIES OF INSURANCE LISTED BELOW HAVE BEEN ISSUED TO THE INSURED NAMED ABOVE FOR THE POLICY PERIOD INDICATED. NOTWITHSTANDING ANY REQUIREMENT, TERM OR CONDITION OF ANY CONTRACT OR OTHER DOCUMENT WITH RESPECT TO WHICH THIS CERTIFICATE MAY BE ISSUED OR MAY PERTAIN, THE INSURANCE AFFORDED BY THE POLICIES DESCRIBED HEREIN IS SUBJECT TO ALL THE TERMS, EXCLUSIONS AND CONDITIONS OF SUCH POLICIES. LIMITS SHOWN MAY HAVE BEEN REDUCED BY PAID CLAIMS.

INSR LTR	TYPE OF INSURANCE	ADDL INSD	SUBR WVD	POLICY NUMBER	POLICY EFF (MM/DD/YYYY)	POLICY EXP (MM/DD/YYYY)	LIMITS
A	<input checked="" type="checkbox"/> COMMERCIAL GENERAL LIABILITY <input type="checkbox"/> CLAIMS-MADE <input checked="" type="checkbox"/> OCCUR <input checked="" type="checkbox"/> Liquor Liability GEN'L AGGREGATE LIMIT APPLIES PER: <input type="checkbox"/> POLICY <input type="checkbox"/> PRO-JECT <input type="checkbox"/> LOC OTHER:			RRG1026821	7/1/2018	7/1/2019	EACH OCCURRENCE \$ \$1,000,000 DAMAGE TO RENTED PREMISES (Ea occurrence) \$ Included MED EXP (Any one person) \$ Included PERSONAL & ADV INJURY \$ Included GENERAL AGGREGATE \$ N/A PRODUCTS - COM/OP AGG \$ Included \$
A	AUTOMOBILE LIABILITY <input type="checkbox"/> ANY AUTO <input checked="" type="checkbox"/> OWNED AUTOS ONLY <input checked="" type="checkbox"/> HIRED AUTOS ONLY <input type="checkbox"/> SCHEDULED AUTOS <input type="checkbox"/> NON-OWNED AUTOS ONLY			RRG1026821 XS1026821	7/1/2018 7/1/2018	7/1/2019 7/1/2019	COMBINED SINGLE LIMIT (Ea accident) \$ \$5,000,000 BODILY INJURY (Per person) \$ BODILY INJURY (Per accident) \$ PROPERTY DAMAGE (Per accident) \$ \$
A	UMBRELLA LIAB <input checked="" type="checkbox"/> EXCESS LIAB DED <input checked="" type="checkbox"/> RETENTION \$ 1,000,000			XS1026821	7/1/2018	7/1/2019	EACH OCCURRENCE \$ \$4,000,000 AGGREGATE \$ \$4,000,000 \$
B	WORKERS COMPENSATION AND EMPLOYERS' LIABILITY ANY PROPRIETOR/PARTNER/EXECUTIVE OFFICER/MEMBER EXCLUDED? (Mandatory in NH) If yes, describe under DESCRIPTION OF OPERATIONS below	Y/N <input type="checkbox"/> N/A		SP 4058841	7/1/2018	7/1/2019	<input checked="" type="checkbox"/> PER STATUTE <input type="checkbox"/> OTH-ER Statutory E.L. EACH ACCIDENT \$ \$1,000,000 E.L. DISEASE - EA EMPLOYEE \$ \$1,000,000 E.L. DISEASE - POLICY LIMIT \$ \$1,000,000

DESCRIPTION OF OPERATIONS / LOCATIONS / VEHICLES (ACORD 101, Additional Remarks Schedule, may be attached if more space is required)
General Liability and Auto Liability limits inclusive of \$250,000 Self-Insured Retention.
If Additional Insured status noted herein, coverage afforded by Form #TNC-G118 (ed. 01/01/12).

For: St. Margaret Mary Church, 119 S Hubbard St, Algonquin, IL 60102.
Reason: Summerfest 2018 to be held at above address on 8/26/18 from 12:00PM to 7:00PM.

CERTIFICATE HOLDER

CANCELLATION

Village of Algonquin
2200 Harnish Dr
Algonquin IL 60102

SHOULD ANY OF THE ABOVE DESCRIBED POLICIES BE CANCELLED BEFORE THE EXPIRATION DATE THEREOF, NOTICE WILL BE DELIVERED IN ACCORDANCE WITH THE POLICY PROVISIONS.

AUTHORIZED REPRESENTATIVE

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Illinois Department of Revenue

Office of Local Government Services
Sales Tax Exemption Section, 3-520
101 W. Jefferson Street
Springfield, Illinois 62702
217 782-8881

June 20, 2014

CATHOLIC CHURCH DIOCESE OF ROCKFORD
555 COLMAN CENTER DR
P O BOX 7044
ROCKFORD IL 61125

We have received your recent letter; and based on the information you furnished, we believe

CATHOLIC CHURCH DIOCESE OF ROCKFORD
of
ROCKFORD, IL

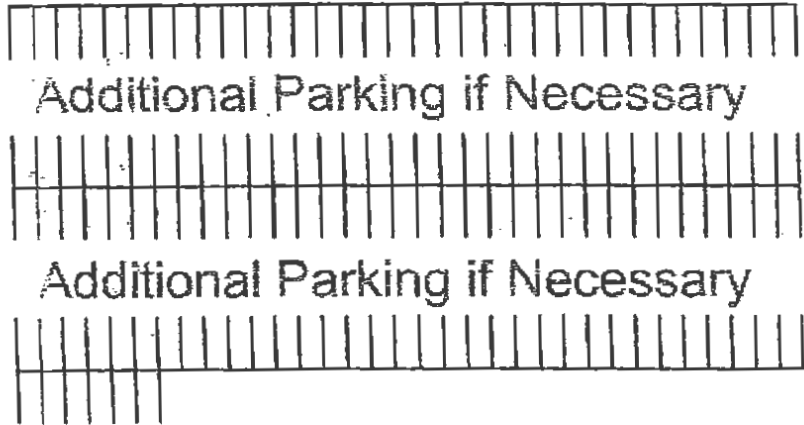
is organized and operated exclusively for religious purposes.

Consequently, sales of any kind to this organization are exempt from the Retailers' Occupation Tax, the Service Occupation Tax (both state and local), the Use Tax, and the Service Use Tax in Illinois.

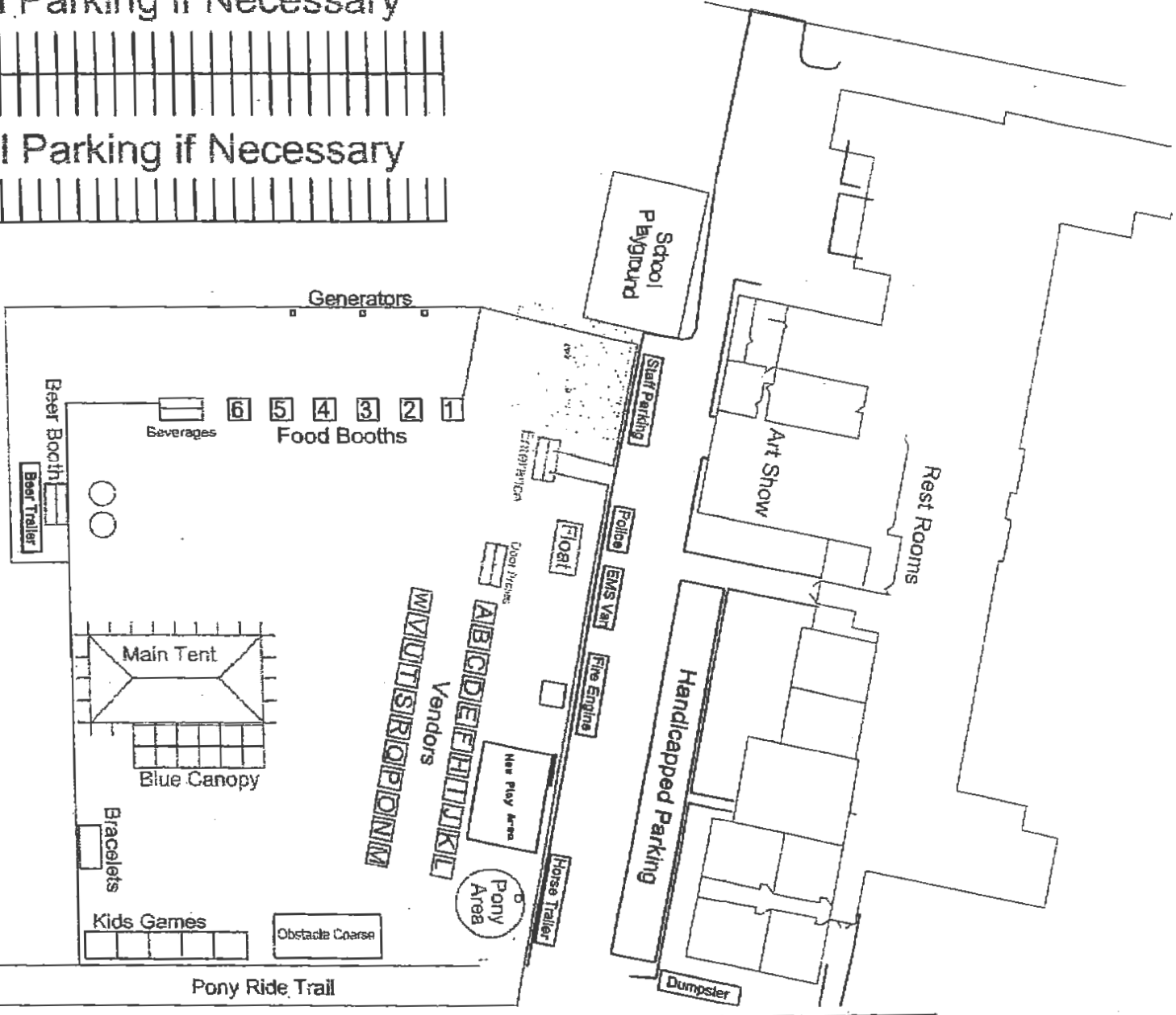
We have issued your organization the following tax exemption identification number: E9991-6673-07. To claim the exemption, you must provide this number to your suppliers when purchasing tangible personal property for organizational use. This exemption may not be used by individual members of the organization to make purchases for their individual use.

This exemption will expire on July 1, 2019, unless you apply to the Illinois Department of Revenue for renewal at least three months prior to the expiration date.

Office of Local Government Services
Illinois Department of Revenue



Additional Parking Drive Way



Eastgate Drive

APPLICATION FOR "EVENT" LIQUOR PERMIT

RECEIVED

TO: The Liquor Commissioner of the Village of Algonquin, Illinois

JUL 10 2018

(PLEASE TYPE OR PRINT ALL INFORMATION)

COMMUNITY
DEVELOPMENT

The undersigned applicant, being duly sworn on oath, makes application for a Liquor Permit in the Village of Algonquin, as follows:

1. The name of the applicant to appear on the permit is: _____
Saint Margaret Mary Church

2. The address of the applicant is: _____
119 S. Hubbard Street, Algonquin, Illinois 60102

3. The name and address of officer or agent for the applicant is: _____
Margaret Przybylko

4. A. The applicant is presently: (Complete all applicable parts)
 - (1) Class _____ Licensee in the Village; License No. _____
 - (2) Nonprofit organization, registered with the State of Illinois
 - (3) Other type of organization: Please specify _____
(i.e., Fraternal, Educational, Civic, Political, Religious)
 - (4) Provide Illinois Department of Revenue Tax Exempt Number and/or Illinois Business Tax Number assigned to your organization _____

B. The description and location of premises or place of business which is to be operated under the proposed permit: _____
Saint Margaret Mary School
119 S. Hubbard Street, Algonquin, IL 60102

C. The date(s) and hours of operation requested under the proposed permit are: _____
August 26, 2018
12:00 PM - 7:00 PM

The number of days shall not exceed what is presently allowed by ordinance.

5. BASSET Training Required: Successful completion of a BASSET program, or other similar program as approved by the Chief of Police, is required for at least one person coordinating and responsible for the responsible sale of alcoholic liquor during the event. Such person

shall be present at the point of liquor sales at all times for the duration of the event.
Person holding the BASSETT Certificate: Hubert Marat

Photocopies of a valid certificate of completion of a BASSET program shall be included with the application.

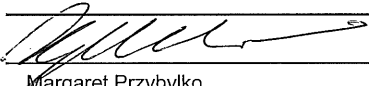
6. The applicant hereby files Certificates of Insurance, certifying that the applicant has in force and effect insurance as follows:
- Liquor Liability Insurance of not less than \$1,000,000.00 per occurrence and \$2,000,000.00 annual aggregate and;
 - General Liability Insurance in an amount not less than \$1,000,000.00 per occurrence and \$2,000,000.00 annual aggregate.

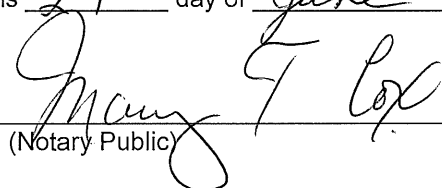
The Insurance Certificates must name the Village of Algonquin as Additional Insured.

"Host Insurance" shall not satisfy the requester defined above.

7. The applicant herewith submits the appropriate permit fee, in the amount of:
\$ 25.00, as set forth in the Liquor Control and Liquor Licensing Ordinance of the Village of Algonquin.

The applicant agrees to comply with all ordinances of the Village of Algonquin and the Laws of the State of Illinois.

Applicant: Saint Margaret Mary Church
Signed By: 
Officer or Agent: Margaret Przybylko
Daytime Phone: 847-658-6071
Extension: _____

Subscribed and Sworn to before me this 27th day of June, 2018

(Notary Public)

My Commission expires 2-5-, 2020.

SEAL



Saint Margaret Mary 2018 Summerfest Security Detail

The following individuals will be providing Security during the 2018 Saint Margaret Mary Summerfest to be held on the parish grounds on August 26, 2018. They will be stationed at the Beer/Wine Tent checking ID's. There will be only one entrance to the tent through this security entrance. (See attached layout of the event.

They have also been asked to walk the entire perimeter to insure that there are not any altercations at the event.

Name

Address

Martin Wojtulewicz

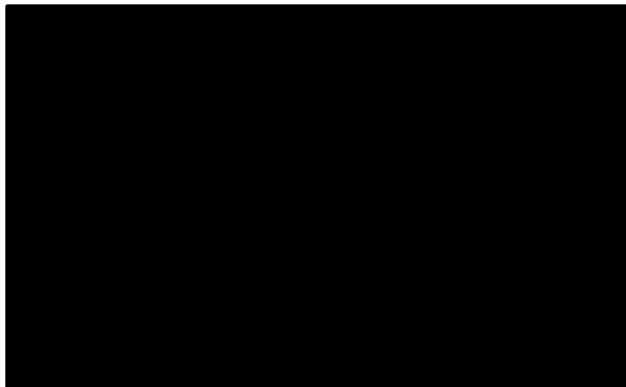
Jerry Kawecki

Hubert Marat

Wally Szyda

Stanislaw Dziadkowiec

Wojciech Lasek



BASSET Card



July 18, 2016



Letter ID: L0104216464

HUBERT MARAT
441 DIAMONDBACK WAY
ALGONQUIN IL 60102

License No.: [REDACTED]
Expiration Date: 7/14/2019
License Type: Basset Card

Your "Student ID number" is: 10023013

Your "Trainer's ID number" is: 5A-0110606

Your BASSET Card is located BELOW

DO NOT throw away this letter as you will need your "Student ID number" directly above to re-print your card.

IMPORTANT:

To re-print your card, visit the Illinois Liquor Control Commission website at ILCC.illinois.gov

(click on the RESOURCES tab to access the "BASSET Card Lookup" page).

<p>ILLINOIS LIQUOR CONTROL COMMISSION 100 W. Randolph Street, Suite 7-801 - Chicago, IL 60601 BEVERAGE ALCOHOL SELLERS AND SERVERS EDUCATION AND TRAINING [BASSET] CARD</p> <p>Date of Certification: 7/14/2016 Expires: 7/14/2019 Trainer's IL Liquor License Number: [REDACTED] HUBERT MARAT [REDACTED]</p> <p>**Card is not transferrable**</p>
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CERTIFICATE OF LIABILITY INSURANCE

DATE (MM/DD/YYYY)

7/16/2018

THIS CERTIFICATE IS ISSUED AS A MATTER OF INFORMATION ONLY AND CONFERS NO RIGHTS UPON THE CERTIFICATE HOLDER. THIS CERTIFICATE DOES NOT AFFIRMATIVELY OR NEGATIVELY AMEND, EXTEND OR ALTER THE COVERAGE AFFORDED BY THE POLICIES BELOW. THIS CERTIFICATE OF INSURANCE DOES NOT CONSTITUTE A CONTRACT BETWEEN THE ISSUING INSURER(S), AUTHORIZED REPRESENTATIVE OR PRODUCER, AND THE CERTIFICATE HOLDER.

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PRODUCER Arthur J. Gallagher Risk Management Services, Inc. 2850 Golf Road Rolling Meadows IL 60008	CONTACT NAME: Gallagher Bassett Services, Inc.		
	PHONE (A/C, No, Ext): 414-203-4053	FAX (A/C, No): 414-258-1250	
INSURED Diocese of Rockford Finance & Administration Office P.O. Box 7044 Rockford IL 61125	INSURER(S) AFFORDING COVERAGE		NAIC #
	INSURER A: National Catholic RRG, Inc		10083
	INSURER B: Safety National Casualty Corporation		15105
	INSURER C:		
	INSURER D:		
	INSURER E:		
INSURER F:			

COVERAGES**CERTIFICATE NUMBER:** 308165777**REVISION NUMBER:**

THIS IS TO CERTIFY THAT THE POLICIES OF INSURANCE LISTED BELOW HAVE BEEN ISSUED TO THE INSURED NAMED ABOVE FOR THE POLICY PERIOD INDICATED. NOTWITHSTANDING ANY REQUIREMENT, TERM OR CONDITION OF ANY CONTRACT OR OTHER DOCUMENT WITH RESPECT TO WHICH THIS CERTIFICATE MAY BE ISSUED OR MAY PERTAIN, THE INSURANCE AFFORDED BY THE POLICIES DESCRIBED HEREIN IS SUBJECT TO ALL THE TERMS, EXCLUSIONS AND CONDITIONS OF SUCH POLICIES. LIMITS SHOWN MAY HAVE BEEN REDUCED BY PAID CLAIMS.

INSR LTR	TYPE OF INSURANCE	ADDL INSD	SUBR WVD	POLICY NUMBER	POLICY EFF (MM/DD/YYYY)	POLICY EXP (MM/DD/YYYY)	LIMITS
A	<input checked="" type="checkbox"/> COMMERCIAL GENERAL LIABILITY <input type="checkbox"/> CLAIMS-MADE <input checked="" type="checkbox"/> OCCUR <input checked="" type="checkbox"/> Liquor Liability GEN'L AGGREGATE LIMIT APPLIES PER: <input type="checkbox"/> POLICY <input type="checkbox"/> PRO-JECT <input type="checkbox"/> LOC OTHER:			RRG1026821	7/1/2018	7/1/2019	EACH OCCURRENCE \$ \$1,000,000 DAMAGE TO RENTED PREMISES (Ea occurrence) \$ Included MED EXP (Any one person) \$ Included PERSONAL & ADV INJURY \$ Included GENERAL AGGREGATE \$ N/A PRODUCTS - COMP/OP AGG \$ Included \$
A	<input checked="" type="checkbox"/> AUTOMOBILE LIABILITY <input type="checkbox"/> ANY AUTO <input checked="" type="checkbox"/> OWNED AUTOS ONLY <input type="checkbox"/> SCHEDULED AUTOS <input checked="" type="checkbox"/> HIRED AUTOS ONLY <input type="checkbox"/> NON-OWNED AUTOS ONLY			RRG1026821 XS1026821	7/1/2018 7/1/2018	7/1/2019 7/1/2019	COMBINED SINGLE LIMIT (Ea accident) \$ \$5,000,000 BODILY INJURY (Per person) \$ BODILY INJURY (Per accident) \$ PROPERTY DAMAGE (Per accident) \$ \$
A	<input type="checkbox"/> UMBRELLA LIAB <input type="checkbox"/> OCCUR <input checked="" type="checkbox"/> EXCESS LIAB <input type="checkbox"/> CLAIMS-MADE <input type="checkbox"/> DED <input checked="" type="checkbox"/> RETENTION \$ 1,000,000			XS1026821	7/1/2018	7/1/2019	EACH OCCURRENCE \$ \$4,000,000 AGGREGATE \$ \$4,000,000 \$
B	<input checked="" type="checkbox"/> WORKERS COMPENSATION AND EMPLOYERS' LIABILITY ANY PROPRIETOR/PARTNER/EXECUTIVE OFFICER/MEMBER EXCLUDED? (Mandatory in NH) If yes, describe under DESCRIPTION OF OPERATIONS below		<input type="checkbox"/> Y <input checked="" type="checkbox"/> N	SP 4058841	7/1/2018	7/1/2019	<input checked="" type="checkbox"/> PER STATUTE <input type="checkbox"/> OTH-ER Statutory E.L. EACH ACCIDENT \$ \$1,000,000 E.L. DISEASE - EA EMPLOYEE \$ \$1,000,000 E.L. DISEASE - POLICY LIMIT \$ \$1,000,000

DESCRIPTION OF OPERATIONS / LOCATIONS / VEHICLES (ACORD 101, Additional Remarks Schedule, may be attached if more space is required)

General Liability and Auto Liability limits inclusive of \$250,000 Self-Insured Retention.
If Additional Insured status noted herein, coverage afforded by Form #TNC-G118 (ed. 01/01/12).

For: St. Margaret Mary Church, 119 S Hubbard St, Algonquin, IL 60102.

Reason: Summerfest 2018 to be held at above address on 8/26/18 from 12:00PM to 7:00PM. Village of Algonquin is named as additional insured and loss payee with respect to this certificate. Dram Shop Liquor Liability coverage applicable to Saint Margaret Mary Parish, Algonquin, IL as additional insured with respect to the event. Included in the Excess/Liability is the liquor liability.

CERTIFICATE HOLDER**CANCELLATION**

Village of Algonquin
2200 Harnish Dr
Algonquin IL 60102

SHOULD ANY OF THE ABOVE DESCRIBED POLICIES BE CANCELLED BEFORE THE EXPIRATION DATE THEREOF, NOTICE WILL BE DELIVERED IN ACCORDANCE WITH THE POLICY PROVISIONS.

AUTHORIZED REPRESENTATIVE

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Village of Algonquin
PUBLIC EVENT/ENTERTAINMENT LICENSE APPLICATION

In order for the Village of Algonquin to assist you with your Public Event, please fill out the information below and return to Diane LaCalamita at the Ganek Municipal Center (2200 Harnish Drive or dlacalamita@algonquin.org) at least 45 days prior to the event.

Please type or print legibly.

Official Name of the Event: JACOBS HOME COMING PARADE

Sponsoring Organization:

Name: SCHOOL DISTRICT 300 Contact Name: TODD ROHLWING
Address: 2550 HARNISH
City, State, ZIP: ALGONQUIN IL 60102
Phone: 847 551 8365 Email: Todd.rohlwing@D300.org

Event Coordinator:

Name: SHELLEY GABOURY KATHY AMES
Home Address: 2550 HARNISH
City, State, ZIP: ALGONQUIN IL 60102
Phone: _____ Email: _____

Event Information:

Describe the Nature of the Event: HOME COMING PARADE FROM WOODCREEK COMMONS TO JACOBS HIGH SCHOOL

New Event ☒ Repeat Event _____ If repeat, will anything be different this year? _____

Event Address: 2601 BUNKER HILL

Date(s) and Time(s) of the Event: 09/23/18 4-5 pm

Rain Date(s), if applicable: _____

Set-Up Date/Time: 09-23-18 3:00pm - 4:00pm

Maximum Number of Attendees/Participants Expected: 200

Admission Fee: Yes _____ No ☒ If Yes, list fee(s) to be charged: _____

How will the revenue be used (include donations to non-profit or charitable organizations): N/A

Event Website: _____

Event Details:

Describe provided security, including who will be providing the security (name and contact information), hours, and a security plan: __

3 POLICE OFFICERS WILL BE HIRED BACK TO STAFF - START,
SHERMAN + BUNKER HILL, AND BUNKER HILL AND GOLDEN EAGLE.

Describe parking or traffic control, including the location of extra parking and the number of spaces allocated, and how overflow parking will be handled: PARKING AT JACOBS.

Will there be a need for road closures? Yes ☒ No _____ If Yes, please explain: TEMPORARY
FOR PARADE - APPROX 15 MINUTES.

Are you requesting Algonquin Police Officer(s) presence? Yes ☒ No _____ If Yes, to perform what function?
TRAFFIC CONTROL

Do you want a fire truck or ambulance present? Yes _____ No ☒ If Yes, for what hours and to perform what function? _____

Are you wishing to post temporary sign(s) announcing the event? Yes ☒ No _____ If Yes, please describe desired size, location and date(s) that the signage will be displayed: _____

PRE EVENT SIGNAGE ON RANDALL AND BUNKER HILL
SPOKE WITH DEPUTY CHIEF MARKHAM ON LOCATIONS

Do you wish to serve alcoholic beverages? Yes _____ No ☒

If Yes, do you have DRAM Shop Insurance for the sale/consumption of alcohol? Yes _____ No ☒ If Yes, attach a copy of the policy.

Will you have live entertainment? (e.g. bands, D.J., amplified sound, etc.) Yes _____ No ☒
If Yes, please describe type, band name(s), and hours of performance and if there will be a stage: _____

PARADE BAND.

Indemnification, Waiver and Release

To be signed by all: applicant, sponsor, organizer, promoter and permittee/licensee.

The Permittee/Licensee shall indemnify and hold harmless the Village, its officers, boards, commissions, agents, elected, elected officials, and employees (collectively, "the Village Indemnitees") from any and all costs, demands, expenses, fees and expenses, arising out of: (a) breach or violation by the Permittee/Licensee of any of its certifications, representations, warranties, covenants or agreements in its application and permit/license issued by the Village; (b) any actual or alleged death or injury to any person, damage to any property or any other damage or loss claimed to result in whole or in part from the negligent performance by or on behalf of the Permittee/Licensee; or (c) any negligent act, activity or omission of permittee or any of its employees, representatives, subcontractors or agents.

The Permittee/Licensee agrees to indemnify, defend and hold harmless the Village Indemnitees against and from any and all losses, claims, demands, causes of action, actions, suits, proceedings, damages, costs and/or liabilities of every kind and nature, whatsoever (including, but not limited to expenses for reasonable legal fees, and disbursements and liabilities assumed by the Village in connection therewith), to persons or property, in any way arising out of or through the acts or omissions of the Permittee/Licensee, its servants, agents or employees, or to which the negligence of the Permittee/Licensee shall in any way contribute.

Permittee/Licensee hereby waives and releases all claims against the Village Indemnitees or arising out of the issuance of a permit to Permittee/Licensee for any and all injuries to persons or damage to property from any cause arising at any time during the event listed herein or the issuance of the Permit/License.

The term "Permittee/Licensee" refers to the applicant, as well as any sponsor, organizer, promoter of the event. Each undersigned represents and warrants that he/she has authority to execute this Indemnification, Waiver and Release Agreement on behalf of the person or entity for which he/she has signed.

Permittee/
Licensee:

Community School District 200

Circle all
that apply:

☒ Applicant ☐ Sponsor ☐ Organizer ☐ Promoter

By:

Susan L. Harker

[Print]

[Signature]

Date:

6-14-18

Do you foresee any other special needs for this event? (Physical set-up assistance, waste removal, portable toilets and hand washing stations, electricity, generator, running water, tent(s), etc.): NO

Do you plan on holding a raffle during this event? Yes _____ No X
(Must be an Algonquin-based, non-profit organization)

Name of on-site contact during the event (please print): BARB WILLE

On-site contact's cell number: _____

On-site contact's work number: _____

On-site contact's home number: _____

Affidavit of Applicant:

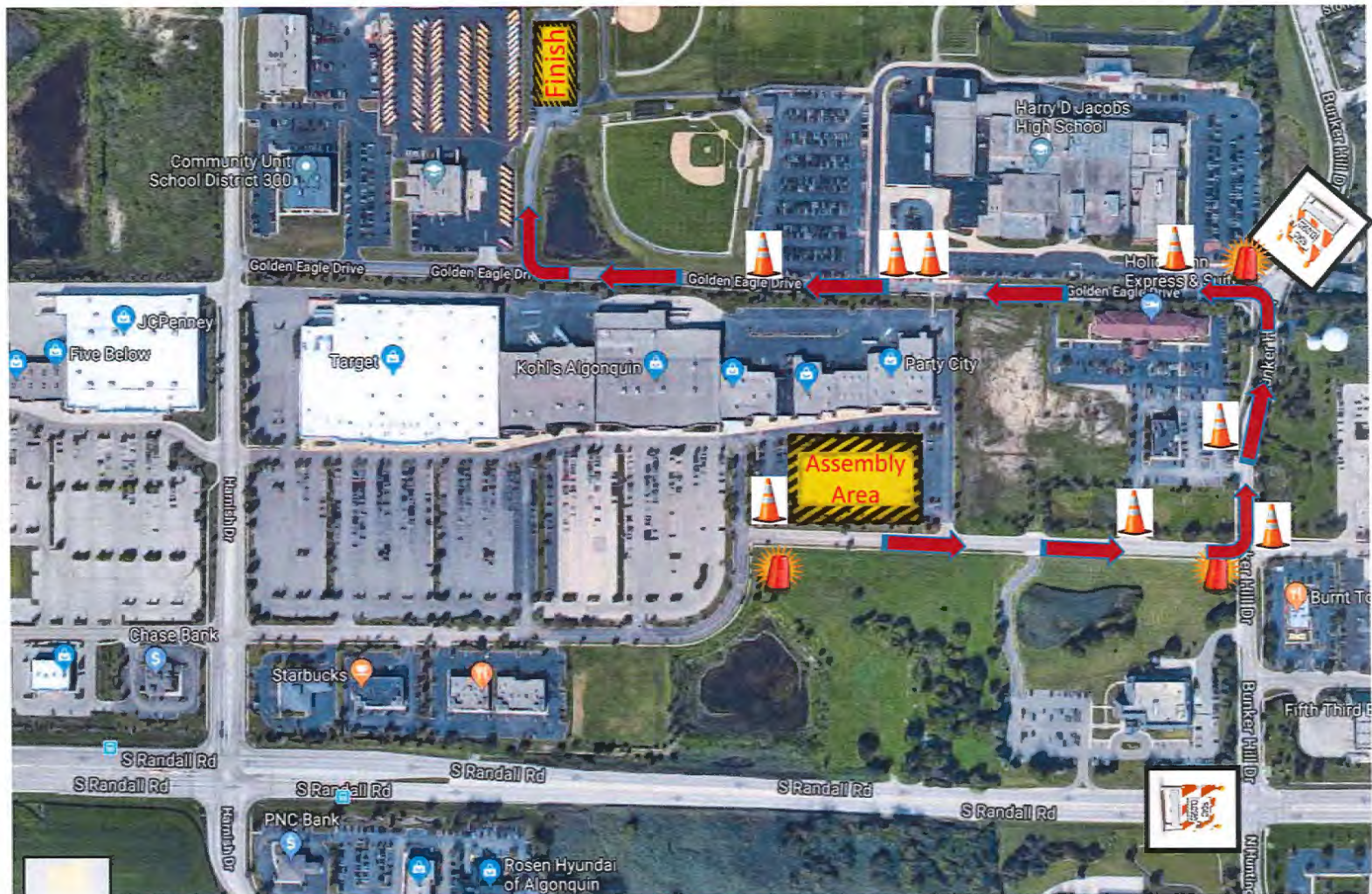
I, the undersigned applicant, or authorized agent of the above noted organization, swear or affirm that the matters stated in the foregoing application are true and correct upon my personal knowledge and information for the purpose of requesting the Village of Algonquin to issue the permit herein applied for, that I am qualified and eligible to obtain the permit applied for and agree to pay all fees, to meet all requirements of the Algonquin Village Code, and any additional regulations, conditions, or restrictions set forth in the permit and to comply with the laws of the Village of Algonquin, the State of Illinois, and the United States of America in the conduct of the Public Event described herein. In addition, Applicant certifies, by signing the application, that, pursuant to 720ILCS 5/11-9.4(c), no sex offenders are employed by the carnival operator, and that no carnival employees are fugitives from Illinois or any other state's law enforcement agencies. I (or the above named organization) further agree(s) to hold harmless and indemnify the Village, its officials, employees and successors and assigns, for any and all liability, damages, suits, claims and demands for damages at law or in equity it incurs as a result and arising either directly or indirectly out of the public event noted above including but not limited to damages and attorney's fees.

Susan L. Hark
Signature of Applicant

6-14-18
Date

Susan L. Hark
Printed Name of Applicant

Harry D Jacobs High School – Parade Route





CERTIFICATE OF LIABILITY INSURANCE

DATE (MM/DD/YYYY)

6/14/2018

THIS CERTIFICATE IS ISSUED AS A MATTER OF INFORMATION ONLY AND CONFERS NO RIGHTS UPON THE CERTIFICATE HOLDER. THIS CERTIFICATE DOES NOT AFFIRMATIVELY OR NEGATIVELY AMEND, EXTEND OR ALTER THE COVERAGE AFFORDED BY THE POLICIES BELOW. THIS CERTIFICATE OF INSURANCE DOES NOT CONSTITUTE A CONTRACT BETWEEN THE ISSUING INSURER(S), AUTHORIZED REPRESENTATIVE OR PRODUCER, AND THE CERTIFICATE HOLDER.

IMPORTANT: If the certificate holder is an ADDITIONAL INSURED, the policy(ies) must have ADDITIONAL INSURED provisions or be endorsed. If SUBROGATION IS WAIVED, subject to the terms and conditions of the policy, certain policies may require an endorsement. A statement on this certificate does not confer rights to the certificate holder in lieu of such endorsement(s).

PRODUCER Arthur J. Gallagher Risk Management Services, Inc. 2850 Golf Road Rolling Meadows IL 60008	CONTACT NAME: Tyler MacKenzie PHONE (A/C, No, Ext): 630-694-5165 E-MAIL ADDRESS: tyler_mackenzie@ajg.com FAX (A/C, No): 630-285-4062														
INSURED Community Unit School District #300 2550 Harnish Drive Algonquin, IL 60102	<table><tr><th>INSURER(S) AFFORDING COVERAGE</th><th>NAIC #</th></tr><tr><td>INSURER A: Collective Liability Insurance Cooperative</td><td></td></tr><tr><td>INSURER B:</td><td></td></tr><tr><td>INSURER C:</td><td></td></tr><tr><td>INSURER D:</td><td></td></tr><tr><td>INSURER E:</td><td></td></tr><tr><td>INSURER F:</td><td></td></tr></table>	INSURER(S) AFFORDING COVERAGE	NAIC #	INSURER A: Collective Liability Insurance Cooperative		INSURER B:		INSURER C:		INSURER D:		INSURER E:		INSURER F:	
INSURER(S) AFFORDING COVERAGE	NAIC #														
INSURER A: Collective Liability Insurance Cooperative															
INSURER B:															
INSURER C:															
INSURER D:															
INSURER E:															
INSURER F:															

COVERAGES**CERTIFICATE NUMBER:** 1964261684**REVISION NUMBER:**

THIS IS TO CERTIFY THAT THE POLICIES OF INSURANCE LISTED BELOW HAVE BEEN ISSUED TO THE INSURED NAMED ABOVE FOR THE POLICY PERIOD INDICATED. NOTWITHSTANDING ANY REQUIREMENT, TERM OR CONDITION OF ANY CONTRACT OR OTHER DOCUMENT WITH RESPECT TO WHICH THIS CERTIFICATE MAY BE ISSUED OR MAY PERTAIN, THE INSURANCE AFFORDED BY THE POLICIES DESCRIBED HEREIN IS SUBJECT TO ALL THE TERMS, EXCLUSIONS AND CONDITIONS OF SUCH POLICIES. LIMITS SHOWN MAY HAVE BEEN REDUCED BY PAID CLAIMS.

INSR LTR	TYPE OF INSURANCE	ADDL SUBR INSD WVD	POLICY NUMBER	POLICY EFF (MM/DD/YYYY)	POLICY EXP (MM/DD/YYYY)	LIMITS
A	<input checked="" type="checkbox"/> COMMERCIAL GENERAL LIABILITY <input type="checkbox"/> CLAIMS-MADE <input checked="" type="checkbox"/> OCCUR <input checked="" type="checkbox"/> Per District Agg GEN'L AGGREGATE LIMIT APPLIES PER: <input type="checkbox"/> POLICY <input type="checkbox"/> PROJECT <input type="checkbox"/> LOC <input type="checkbox"/> OTHER:		CLICGGL2017	7/1/2017	7/1/2016	EACH OCCURRENCE \$ 1,000,000 DAMAGE TO RENTED PREMISES (Ea occurrence) \$ 500,000 MED EXP (Any one person) \$ Excluded PERSONAL & ADV INJURY \$ 1,000,000 GENERAL AGGREGATE \$ 3,000,000 PRODUCTS - COMP/OP AGG \$ Included \$
A	AUTOMOBILE LIABILITY <input checked="" type="checkbox"/> ANY AUTO <input type="checkbox"/> OWNED AUTOS ONLY <input type="checkbox"/> SCHEDULED AUTOS <input checked="" type="checkbox"/> HIRED AUTOS ONLY <input checked="" type="checkbox"/> NON-OWNED AUTOS ONLY <input checked="" type="checkbox"/> Compl/Coll <input checked="" type="checkbox"/> \$2,500 Ded		CLICAL2017 CLICPR2017	7/1/2017 7/1/2017	7/1/2016 7/1/2016	COMBINED SINGLE LIMIT (Ea accident) \$ 2,000,000 BODILY INJURY (Per person) \$ BODILY INJURY (Per accident) \$ PROPERTY DAMAGE (Per accident) \$ Auto Physical Dmg. \$ 2,500 Dist. Ded
A	<input type="checkbox"/> UMBRELLA LIAB <input checked="" type="checkbox"/> OCCUR <input checked="" type="checkbox"/> EXCESS LIAB <input type="checkbox"/> CLAIMS-MADE <input type="checkbox"/> DED <input checked="" type="checkbox"/> RETENTION \$ 0		CLICXS2017	7/1/2017	7/1/2016	EACH OCCURRENCE \$ 10,000,000 AGGREGATE \$ 10,000,000 \$
A	WORKERS COMPENSATION AND EMPLOYERS' LIABILITY ANY PROPRIETOR/PARTNER/EXECUTIVE OFFICER/MEMBER EXCLUDED? (Mandatory in NH) If yes, describe under DESCRIPTION OF OPERATIONS below	Y/N <input type="checkbox"/> N/A	CLICWC2017	7/1/2017	7/1/2016	<input checked="" type="checkbox"/> PER STATUTE <input type="checkbox"/> OTHER E.L. EACH ACCIDENT \$ 2,000,000 E.L. DISEASE - EA EMPLOYEE \$ 2,000,000 E.L. DISEASE - POLICY LIMIT \$ 2,000,000

DESCRIPTION OF OPERATIONS / LOCATIONS / VEHICLES (ACORD 101, Additional Remarks Schedule, may be attached if more space is required)

*Collective Liability Insurance Cooperative Reinsurers - Great American Insurance Company (NAIC #16691), Pennsylvania Manufacturers' Association Insurance Company (NAIC #12262), Lloyd's Syndicate 2987, Collective Liability Insurance Cooperative Excess Workers Compensation Insurer - Safety National Casualty Corporation (NAIC #15105).

Additional Insured status provided herein afforded by CLIC CG2028 0413, CLIC CG2012 0413, CLIC CG2013 0413, CLIC CG2026 0413, CLIC CG2015 0413 and/or CA CLIC 0715 when applicable.
Village of Algonquin is shown as Additional Insured solely as respects to General Liability coverage evidenced herein regarding the use of facilities by the Named Insured during the policy period.
See Attached...

CERTIFICATE HOLDER**CANCELLATION**

Village of Algonquin
2200 Harnish Drive
Algonquin IL 60102
USA

SHOULD ANY OF THE ABOVE DESCRIBED POLICIES BE CANCELLED BEFORE THE EXPIRATION DATE THEREOF, NOTICE WILL BE DELIVERED IN ACCORDANCE WITH THE POLICY PROVISIONS.

AUTHORIZED REPRESENTATIVE

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THIS ENDORSEMENT CHANGES THE COVERAGE DOCUMENT. PLEASE READ IT CAREFULLY.

ADDITIONAL MEMBER – DESIGNATED PERSON OR ORGANIZATION

This endorsement modifies coverage provided under the following:

COMMERCIAL GENERAL LIABILITY COVERAGE PART

SCHEDULE

State Or Governmental Agency Or Subdivision Or Political Subdivision: Any person or organization if required by contract or agreement, or as described on a Certificate of Insurance issued by us or other authorized person or organization.
--

Information required to complete this Schedule, if not shown above, will be shown in the Declarations.
--

A. Section II – Who Is A Member is amended to include as an Additional Member the person(s) or organization(s) shown in the Schedule, but only with respect to liability for "bodily injury", "property damage" or "personal and advertising injury" caused, in whole or in part, by your acts or omissions or the acts or omissions of those acting on your behalf:

1. In the performance of your ongoing operations; or
2. In connection with your premises owned by or rented to you.

However:

1. The coverage afforded to such Additional Member only applies to the extent permitted by law; and
2. If coverage provided to the Additional Member is required by a contract or agreement, the coverage afforded such Additional Member will not be broader than that which you are required by the contract or agreement to provide for such Additional Member.

B. With respect to the coverage afforded to these Additional Members, the following is added to Section III – Limits of Coverage:

If coverage provided to the Additional Member is required by a contract or agreement, the most we will pay on behalf of the Additional Member is the amount of coverage available under the applicable Limits of Coverage shown in the Declarations.

This endorsement shall not increase the applicable Limits of Coverage shown in the Declarations.

Verify that all of your Illinois Sales Tax Exemption Certificate information is correct

- ✓ If not, contact us immediately.
- ✓ **Do not discard** - your Illinois Sales Tax Exemption Certificate is an important tax document that authorizes you to purchase tangible personal property for use or consumption tax-free.

Illinois Sales Tax Exemption Certificate

COMMUNITY UNIT SCHOOL DISTRICT #300

2550 HARNISH DR
ALGONQUIN IL 60102-6870

Sales Tax Exemption Certificate

Issue date: 01/02/2015	Sales Tax Exemption	E99960079
Expiration date: 03/01/2020	Organization type:	Governmental

This entity is authorized under the Retailers' Occupation Tax Act to purchase tangible personal property for use or consumption tax-free.

 *Carver Beard*
Director
DEPARTMENT OF REVENUE



Village of Algonquin
PUBLIC EVENT/ENTERTAINMENT LICENSE APPLICATION

In order for the Village of Algonquin to assist you with your Public Event, please fill out the information below and return to Diane LaCalamita at the Ganek Municipal Center (2200 Harnish Drive or dlaalamita@algonquin.org) at least 45 days prior to the event.

Please type or print legibly.

Official Name of the Event: Algonquin Aces Fall Fling

Sponsoring Organization:

Name: Algonquin Area Youth Org. Contact Name: Larry Eissler
Address: P.O. Box 265
City, State, ZIP: Algonquin IL 60102
Phone: [REDACTED] Email: [REDACTED]

Event Coordinator:

Name: Larry + Sheila Eissler
Home Address: [REDACTED]
City, State, ZIP: [REDACTED]
Phone: [REDACTED] Email: [REDACTED]

Event Information:

Describe the Nature of the Event: Fastpitch Softball Tournament for girls
ages 12 thru 16 hosted by AAYO + the Algonquin
Aces Travel Softball Teams

New Event ☐ Repeat Event ☒ If repeat, will anything be different this year? No

Event Address: Kelliher Park, Presidential Park + Algonquin Lakes Park

Date(s) and Time(s) of the Event: Sat Sept 29 9AM - 7pm + Sun Sept 30 7AM - 7pm

Rain Date(s), if applicable: —

Set-Up Date/Time: Fri Sept 28 4pm

Maximum Number of Attendees/Participants Expected: approx 250 per location at different times

Admission Fee: Yes ☐ No ☒ If Yes, list fee(s) to be charged: —

How will the revenue be used (include donations to non-profit or charitable organizations): Proceeds help
with the Aces Softball program along with covering equipment
needed + to help offset World Series costs for the Teams.

Event Website: WWW.AlgonquinAces.COM

Event Details:

Describe provided security, including who will be providing the security (name and contact information), hours, and a security plan: _____

Describe parking or traffic control, including the location of extra parking and the number of spaces allocated, and how overflow parking will be handled: plenty of parking at Algonquin Lakes,

Side Streets overflow parking at Presidential, Tennis courts area

+ Bike path area are overflow parking at Kelliher.

Will there be a need for road closures? Yes _____ No ☒ If Yes, please explain: _____

Are you requesting Algonquin Police Officer(s) presence? Yes _____ No ☒ If Yes, to perform what function? _____

Do you want a fire truck or ambulance present? Yes _____ No ☒ If Yes, for what hours and to perform what function? _____

Are you wishing to post temporary sign(s) announcing the event? Yes _____ No ☒ If Yes, please describe desired size, location and date(s) that the signage will be displayed: _____

Do you wish to serve alcoholic beverages? Yes _____ No ☒

If Yes, do you have DRAM Shop Insurance for the sale/consumption of alcohol? Yes _____ No _____ If Yes, attach a copy of the policy.

Will you have live entertainment? (e.g. bands, D.J., amplified sound, etc.) Yes _____ No ☒
If Yes, please describe type, band name(s), and hours of performance and if there will be a stage: _____

Do you foresee any other special needs for this event? (Physical set-up assistance, waste removal, portable toilets and hand washing stations, electricity, generator, running water, tent(s), etc.):

Garbage removed Sun + Mon Morning - Bathrooms Stocked Friday night
or Sat morning + Sunday morning

Do you plan on holding a raffle during this event? Yes _____ No ✓
(Must be an Algonquin-based, non-profit organization)

Name of on-site contact during the event (please print):

Larry Eissler

On-site contact's cell number:

On-site contact's work number:

On-site contact's home number:

Affidavit of Applicant:

I, the undersigned applicant, or authorized agent of the above noted organization, swear or affirm that the matters stated in the foregoing application are true and correct upon my personal knowledge and information for the purpose of requesting the Village of Algonquin to issue the permit herein applied for, that I am qualified and eligible to obtain the permit applied for and agree to pay all fees, to meet all requirements of the Algonquin Village Code, and any additional regulations, conditions, or restrictions set forth in the permit and to comply with the laws of the Village of Algonquin, the State of Illinois, and the United States of America in the conduct of the Public Event described herein. In addition, Applicant certifies, by signing the application, that, pursuant to 720ILCS 5/11-9.4(c), no sex offenders are employed by the carnival operator, and that no carnival employees are fugitives from Illinois or any other state's law enforcement agencies. I (or the above named organization) further agree(s) to hold harmless and indemnify the Village, its officials, employees and successors and assigns, for any and all liability, damages, suits, claims and demands for damages at law or in equity it incurs as a result and arising either directly or indirectly out of the public event noted above including but not limited to damages and attorney's fees.

Larry Eissler

Signature of Applicant

Larry Eissler

Printed Name of Applicant

6-25-18

Date



CERTIFICATE OF LIABILITY INSURANCE

DATE (MM/DD/YYYY)
4/25/2018

THIS CERTIFICATE IS ISSUED AS A MATTER OF INFORMATION ONLY AND CONFERS NO RIGHTS UPON THE CERTIFICATE HOLDER. THIS CERTIFICATE DOES NOT AFFIRMATIVELY OR NEGATIVELY AMEND, EXTEND OR ALTER THE COVERAGE AFFORDED BY THE POLICIES BELOW. THIS CERTIFICATE OF INSURANCE DOES NOT CONSTITUTE A CONTRACT BETWEEN THE ISSUING INSURER(S), AUTHORIZED REPRESENTATIVE OR PRODUCER, AND THE CERTIFICATE HOLDER.

IMPORTANT: If the certificate holder is an ADDITIONAL INSURED, the policy(ies) must be endorsed. If SUBROGATION IS WAIVED, subject to the terms and conditions of the policy, certain policies may require an endorsement. A statement on this certificate does not confer rights to the certificate holder in lieu of such endorsement(s).

PRODUCER Dawson Insurance Agency 303 E. Main St. Ste. 203 Barrington IL 60010	CONTACT NAME: Tim PHONE (A/C, No, Ext): 847-658-5644 E-MAIL ADDRESS: t.burke@dawsoninsure.com FAX (A/C, No): 847-658-1283
INSURED Algonquin Area Youth Organization P O Box 265 Algonquin IL 60102	INSURER(S) AFFORDING COVERAGE INSURER A : Indiana Ins. Companies INSURER B : INSURER C : INSURER D : INSURER E : INSURER F : NAIC # 22659

COVERAGES **CERTIFICATE NUMBER:** 1831710215 **REVISION NUMBER:**

THIS IS TO CERTIFY THAT THE POLICIES OF INSURANCE LISTED BELOW HAVE BEEN ISSUED TO THE INSURED NAMED ABOVE FOR THE POLICY PERIOD INDICATED. NOTWITHSTANDING ANY REQUIREMENT, TERM OR CONDITION OF ANY CONTRACT OR OTHER DOCUMENT WITH RESPECT TO WHICH THIS CERTIFICATE MAY BE ISSUED OR MAY PERTAIN, THE INSURANCE AFFORDED BY THE POLICIES DESCRIBED HEREIN IS SUBJECT TO ALL THE TERMS, EXCLUSIONS AND CONDITIONS OF SUCH POLICIES. LIMITS SHOWN MAY HAVE BEEN REDUCED BY PAID CLAIMS.

INSR LTR	TYPE OF INSURANCE	ADDL SUBR INSR WVD	POLICY NUMBER	POLICY EFF (MM/DD/YYYY)	POLICY EXP (MM/DD/YYYY)	LIMITS
A	GENERAL LIABILITY <input checked="" type="checkbox"/> COMMERCIAL GENERAL LIABILITY CLAIMS-MADE <input checked="" type="checkbox"/> OCCUR GEN'L AGGREGATE LIMIT APPLIES PER: POLICY <input type="checkbox"/> PRO-JECT <input type="checkbox"/> LOC		BK057726771	4/24/2018	4/24/2019	EACH OCCURRENCE \$ 2,000,000 DAMAGE TO RENTED PREMISES (Ea occurrence) \$ 300,000 MED EXP (Any one person) \$ 15,000 PERSONAL & ADV INJURY \$ 2,000,000 GENERAL AGGREGATE \$ 4,000,000 PRODUCTS - COM/OP AGG \$ 4,000,000 \$
A	AUTOMOBILE LIABILITY ANY AUTO ALL OWNED AUTOS HIRED AUTOS SCHEDULED AUTOS NON-OWNED AUTOS <input checked="" type="checkbox"/> <input checked="" type="checkbox"/>		BAS57726771	4/24/2018	4/24/2019	COMBINED SINGLE LIMIT (Ea accident) \$ 1,000,000 BODILY INJURY (Per person) \$ BODILY INJURY (Per accident) \$ PROPERTY DAMAGE (Per accident) \$ \$
A	UMBRELLA LIAB EXCESS LIAB DED <input checked="" type="checkbox"/> RETENTION \$ 10,000 <input checked="" type="checkbox"/> OCCUR CLAIMS-MADE		US057726771	4/24/2018	4/24/2019	EACH OCCURRENCE \$ 1,000,000 AGGREGATE \$ \$
A	WORKERS COMPENSATION AND EMPLOYERS' LIABILITY ANY PROPRIETOR/PARTNER/EXECUTIVE OFFICER/MEMBER EXCLUDED? (Mandatory in NH) If yes, describe under DESCRIPTION OF OPERATIONS below Y/N <input type="checkbox"/> N/A		XW057726771	4/28/2018	4/28/2019	<input checked="" type="checkbox"/> WC STATUTORY LIMITS <input type="checkbox"/> OTHER E.L. EACH ACCIDENT \$ 500,000 E.L. DISEASE - EA EMPLOYEE \$ 500,000 E.L. DISEASE - POLICY LIMIT \$ 500,000

DESCRIPTION OF OPERATIONS / LOCATIONS / VEHICLES (Attach ACORD 101, Additional Remarks Schedule, if more space is required)
ALL EVENTS

CERTIFICATE HOLDER

VILLAGE OF ALGONQUIN
2200 Harnish Dr.
Algonquin IL 60102

CANCELLATION

SHOULD ANY OF THE ABOVE DESCRIBED POLICIES BE CANCELLED BEFORE THE EXPIRATION DATE THEREOF, NOTICE WILL BE DELIVERED IN ACCORDANCE WITH THE POLICY PROVISIONS.

AUTHORIZED REPRESENTATIVE

Timothy J. Burke

© 1988-2010 ACORD CORPORATION. All rights reserved.

Indemnification, Waiver and Release

To be signed by all: applicant, sponsor, organizer, promoter and permittee/licensee.

The Permittee/Licensee shall indemnify and hold harmless the Village, its officers, boards, commissions, agents, elected, elected officials, and employees (collectively, "the Village Indemnitees") from any and all costs, demands, expenses, fees and expenses, arising out of: (a) breach or violation by the Permittee/Licensee of any of its certifications, representations, warranties, covenants or agreements in its application and permit/license issued by the Village; (b) any actual or alleged death or injury to any person, damage to any property or any other damage or loss claimed to result in whole or in part from the negligent performance by or on behalf of the Permittee/Licensee; or (c) any negligent act, activity or omission of permittee or any of its employees, representatives, subcontractors or agents.

The Permittee/Licensee agrees to indemnify, defend and hold harmless the Village Indemnitees against and from any and all losses, claims, demands, causes of action, actions, suits, proceedings, damages, costs and/or liabilities of every kind and nature, whatsoever (including, but not limited to expenses for reasonable legal fees, and disbursements and liabilities assumed by the Village in connection therewith), to persons or property, in any way arising out of or through the acts or omissions of the Permittee/Licensee, its servants, agents or employees, or to which the negligence of the Permittee/Licensee shall in any way contribute.

Permittee/Licensee hereby waives and releases all claims against the Village Indemnitees or arising out of the issuance of a permit to Permittee/Licensee for any and all injuries to persons or damage to property from any cause arising at any time during the event listed herein or the issuance of the Permit/License.

The term "Permittee/Licensee" refers to the applicant, as well as any sponsor, organizer, promoter of the event. Each undersigned represents and warrants that he/she has authority to execute this Indemnification, Waiver and Release Agreement on behalf of the person or entity for which he/she has signed.

Permittee/
Licensee: Algonguin Area Youth Organization
Larry Eissler

Circle all
that apply: Applicant Sponsor Organizer Promoter

By: Larry Eissler
[Print]
Larry Eissler
[Signature]

Date: 6-25-18



VILLAGE OF ALGONQUIN
COMMUNITY DEVELOPMENT DEPARTMENT

– M E M O R A N D U M –

DATE: July 24, 2018

TO: Committee of the Whole

FROM: Benjamin A. Mason, AICP, Senior Planner

SUBJECT: Case No. 2018-09. CarMax – Re-zoning, Final Plat, Final PUD & SUP
(motor vehicle sales and service)

Introduction

Greg Saia, Centerpoint Integrated Solutions, has submitted a petition on behalf of CarMax Auto Superstores, to redevelop the former Duro-Life Manufacturing property at 2401 Huntington Drive North. Enclosed is a map of the subject property, located at the southeast corner of S. Randall Road and Huntington Drive. The site has been vacant for several years as the property owner first tried to lease the building to a manufacturer who could re-use the existing building. The property consists of 13.80 acres and is currently zoned I-1, Industrial.

Actions Requested

Re-zoning

The property was zoned I-1, Industrial when Duro-Life first developed the property several decades ago, and that legacy zoning remains intact which would allow a new light industrial / manufacturing business to occupy the site. I-1, Industrial would allow for automotive uses with a Special Use Permit, however staff has requested – and CarMax has agreed – that the subject property be down-zoned to B-2, Business (General Retail) which would be more appropriate and compatible with the retail and office commercial uses that have developed over time in the immediate vicinity.

As noted in the review memo enclosed from Teska Associates, the Village's Future Land Use Map shows this site designated for business park/light industrial, based on the prior existing Duro-Life development. Absent the legacy manufacturing use, the property would most likely have been planned for predominantly commercial retail use consistent with the adjacent properties both north and south along Randall Road.

To clarify, CarMax will only be purchasing the parcel where the former Duro-Life building is located; the adjacent 10-acre undeveloped commercial property to the east that abuts Stonegate Road is not part of this petition (see the enclosed subject property map).

Plat of Subdivision

The DuroLife building parcel will be subdivided into three lots: Lot 1 will be 6.7 acres in size and developed by CarMax; Lot 2 will be 4.8 acres in size and available for a future commercial use; and Lot 3 will be 2.2 acres in size and consist of a stormwater detention pond to serve the site. A cross-access easement is provided on the east side of Lot 1 to allow a future connection to the surplus Lot 2 commercial parcel. The stormwater detention basin on Lot 3 will be deeded to the Village after it has been completed to village standards and specifications.

Planned Unit Development

The petition involves Final PUD approval for the CarMax proposed development on Lot 1 and stormwater detention on Lot 3. Lot 2 shall be required to come back in for review by the Planning and Zoning Commission and Village Board for Final PUD approval at the time plans are submitted to develop that future commercial site. CarMax does not have any plans to develop Lot 2 itself, rather they will intend to simply market the property and sell to another developer.

Special Use Permit

Lastly, the petitioner is requesting a Special Use Permit for automotive sales and service, which could be issued today under the current I-1 zoning; however, again it is important to note CarMax has agreed to downzone the property to B-2, Business.

Staff / Consultant Comments

Site Plan / Operations

The CarMax sales showroom and service building will be located toward the center of the property, and their main entrances will be oriented toward Randall Road and Huntington Drive. Parking for customers and employees will be located north of the building and be physically separated by security gates from the sales display area which will be located west of the building facing Randall Road. An internal sales / service staging area will be located at the rear of the building, and be enclosed by a six-foot tall masonry wall to provide screening and security.

Though village code does not offer a recommended ratio for required number of parking spaces for an automotive dealership, there is ample space on site south of the proposed customer / employee parking lot to expand if needed and is designated on the site plan as “Future Parking Held in Reserve”.

CarMax will re-use the existing entrance to the property along Huntington Drive. A second right-in only entrance will be added across from Rolls Drive. CarMax anticipates 4-6 tractor trailer deliveries of vehicles per day and a designated parking and unloading area is designated on-site within the customer and employee parking area. The McHenry County Division of Transportation will also grant a right-in/right-out access at the southern boundary of Lot 2, which a future developer of that commercial site would have the ability to construct as part of their development plans.

It is important to consider the intensity of the previous Duro-Life operations when evaluating the proposed re-use of the site for the CarMax business. Specifically, Duro-Life had approximately 50 employees and the standard work week toward the end of their occupancy of the building was two, 10 hour shifts, five days a week with occasional Saturdays. At their peak, Duro-Life operated 24 hours a day, 7 days a week and employed 100 workers. The business averaged a minimum of 15 trucks per day through shipping and receiving (e.g., semi-trailers, flatbed trucks, smaller commercial trucks) in addition to one scrap truck pickup nightly. This previous manufacturing use of the property could be re-established under current zoning and within the former Duro-Life building without further review by the Village.

CarMax estimates this location will have 25 employees. Sales hours are anticipated to be 9:00am to 9:00pm, Monday through Saturday. Service hours will be approximately 7:30am to 6:00pm, Monday through Friday. Staff recommends similar conditions be applied to CarMax that were required of Rosen Hyundai when that dealership obtained PUD approval, to include:

- ✓ Full illumination of the buildings and parking areas shall be limited to one hour after the close of business, or 10:00pm, whichever occurs first;
- ✓ Automobile display area is limited to paved parking surfaces only, and no vehicle shall be placed on an elevated rack nor displayed with an open hood, doors, or trunk lid. No sales vehicles shall be allowed for display in the customer and employee parking lot;
- ✓ CarMax shall advise its customers and employees that test driving and mechanics' drive patterns shall not occur on Huntington Drive east of the subject property. CarMax shall prepare a test drive map exhibit to be approved by the Village and, after approval, to be handed out to its customers. The site exit to Huntington Drive shall be signed to indicate that test drives and vehicle carriers shall turn left only;
- ✓ The following signs are prohibited: Inflatable signs, flags, pennants, banners, any sign with flashing or moving lights, search lights, and signs or stringers that move, excluding "Grand Opening" and "Now Hiring" banners which shall comply with the Village's temporary sign requirements;
- ✓ Reasonable effort shall be made to keep service doors closed at all times except when a vehicle is entering or exiting the building, or when determined by CarMax management that the doors should remain open for extraordinary reasons;
- ✓ No storage of tires, parts, fluids or any materials, goods or waste products of any kind is allowed outside of an approved storage structure or building, and no goods or materials shall be stored higher than the walls of any outside storage area. All automobile fluids, including oil, grease and antifreeze must be stored and disposed of in accordance with all applicable regulations;
- ✓ No inoperable or disassembled vehicle may be stored in the customer parking lot or sales area at any time and all work conducted on vehicles must occur within the service center;
- ✓ The business hours for the service center shall be no earlier than 7:30am, and no later than 9:00pm;

- ✓ The dealership shall educate its employees to refrain from using vehicle horns except in an emergency. Exterior loud speakers / paging systems shall be prohibited except for security / emergency purposes.

In addition to the high quality site and design standards CarMax will be subject to as part of a Planned Unit Development approval, the above conditions regulating operations will further establish the compatibility of the proposed use with both the adjacent commercial businesses and residential subdivisions to the east.

Traffic Study

The CarMax site on Lot 1 will be accessed by the existing full-access entrance / exit – site access A – that previously served as the main driveway for Duro-Life off Huntington Drive. Additionally, a right-in only is proposed across from Rolls Drive – site access B – to allow customers and vehicle carriers to enter the property without having to drive further east down Huntington to access the existing entrance.

The traffic study submitted by the petitioner estimates the overwhelming majority of trips to and from the site will be generated from Randall Road, approximately fifty percent coming up from the south and the other fifty percent down from the north (see pp. 14 of Traffic Impact Study by Kimley Horn enclosed). Approximately 15 to 20 vehicles per hour at peak times are projected to enter the site, distributed between the two access points A & B (see Exhibit 3 of Traffic Impact Study). Since the automotive dealership serves more as a regional destination, rather than a neighborhood commercial convenience, there will be a dedicated left-turn out of the site and a shared straight / right-turn exit lane for the small number of customers who may reside in the local area and therefore access the business via the local public road network.

Currently, the intersection of Randall and Huntington was determined by the traffic study to operate at a Level of Service (LOS) C in the weekday morning peak and LOS D in the weekday afternoon peak and mid-day on Saturday. LOS C is defined as formation of queues that contribute to lower average travel speeds, and LOS D is defined as considerable delays with a continuing decrease in travel speeds. While both LOS C and D categories are still considered to be acceptable by most transportation agencies – LOS E and LOS F are the lowest ratings that signify a roadway is saturated or over-saturated – it is important to note the considerable improvements to the Randall and Huntington intersection that the McHenry County Division of Transportation (MCDOT) is slated to begin work on soon.

Currently the westbound Huntington Drive approach at Randall Road consists of a single dedicated left turn lane and one shared through / right-turn lane. MCDOT's design for widening the approach includes an increase to two dedicated left turn lanes, two through lanes and the addition of a dedicated right turn lane. The traffic study projects a LOS C at all peak times following the county's planned improvements to Huntington, and a LOS B for the traffic flow and operations of the entrance and exits to the CarMax property. To achieve this significant road improvement, CarMax has indicated it is willing to deed

MCDOT the land necessary at the north end of their proposed Lot 1 for the widening. Overall, the county's investment to improve the intersection, coupled with CarMax's willing participation to grant the necessary right-of-way, will be of considerable benefit to the Village and its local road network.

Architecture

CarMax's sales and service building will feature a predominantly masonry exterior with brick on all four sides and a split faced concrete block foundation in a complementary dark earthtone color. A lighter brick will be used for horizontal banding that will wrap around all four sides and help break up the massing of the building. EIFS material will be used as an accent on the canopy columns that define the customer entrances into the building. A six-foot tall masonry wall will surround the south side of the building and staging area located at the rear of the building; the wall shall be the same color and brick material as the exterior of the building itself.

The relative size and scale of CarMax's proposed location here in Algonquin is substantially smaller than their other facilities in the surrounding suburbs, please see Table 1 below:

Table 1	<i>Algonquin - proposed</i>	Naperville CarMax	Schaumburg CarMax
Building Size	<i>7,400 square feet</i>	60,000 square feet	87,000 square feet
Property Acreage	<i>6.70 acres</i>	13 acres	22 acres

A significant reason for the difference in size of CarMax's other locations is their Naperville and Schaumburg locations function as production facilities where vehicles are comprehensively re-conditioned and repaired prior to being made available for sale. The dealership in Algonquin will simply be a sales center, where vehicles arrive ready-for-showroom and maintenance is limited to routine servicing of customer vehicles, such as oil changes, tire rotation, brakes and other minor upkeep.

Signage

The petitioner is proposing two monument signs, one at the intersection of Randall and Huntington, and a second further east along Huntington Drive closer to the entrance to the property. Per village code, a maximum of one monument sign is permitted for the development. The monument sign shall comply with the Village's Sign Code regulations, which include a maximum height of 10 feet from grade and surface area maximum for signable copy of 128 square feet if double-faced. The entire perimeter and base of the monument sign shall be masonry to match the brick material on the building and a decorative stone cap shall be added to the top of the sign. As an alternative to a second monument sign, Staff proposes and would support smaller entrance signage at the



two access locations along Huntington Drive, similar to the sign panels that Rosen Hyundai has on the brick pillars adjacent to their driveway along Harnish Drive (see photo at right).

Engineering Plans

The site engineering plans show extensive grading work proposed on the property, to provide a more gradual slope throughout the site and relatively flat surface along the Randall Road frontage on Lot 1 for the CarMax sales display area. The existing pond at the northwest corner of the site will be filled in, as it was simply an aesthetic feature and did not have a stormwater detention function. Detention for the CarMax development on Lot 1 is provided by a new detention pond at the eastern edge of the site, on proposed Lot 3. While there is an opportunity for CarMax to direct their Lot 1 stormwater runoff directly south into the wetland complex the Village recently purchased, CarMax has indicated it needs the additional dirt / fill from digging out the proposed pond on Lot 3 to balance the site elsewhere and achieve their grading objectives.

Village Staff is comfortable with CarMax's overall stormwater approach that will direct runoff into a pond on Lot 3, however the Village Engineer has been asked to recommend to the developer some options for redesigning the rectangular pond into a more naturalized looking basin that is less linear and potentially moved further off the eastern lot line.

Shifting the stormwater pond west would also allow more creative and curvilinear options for routing a new bike path that will extend down from Huntington Drive to the southern lot line of Lot 2. The path will tie-in to the planned Randall Road underpass / path that will be sited approximately 10-feet south of Lot 2 on Village-owned property and ultimately connect Stonegate Road on the east to Sherman Road and Bunker Hill Drive on the west side of Randall Road. Enclosed is an exhibit that shows the location of the Randall Road underpass / path and how it relates to the property CarMax is purchasing.

Photometric Plan

The parking lot lighting will consist of LED fixtures and are shown on the enclosed plans to be appropriately downcast. The average foot candle readings of 9.07 in the sales display area is considered reasonable for an automotive dealership; for reference purposes, lighting industry standards recommend an average of 10.0 foot candle readings for gasoline service stations. CarMax is proposing lower average readings around 4 foot candles for the customer and employee parking lot along Huntington Drive. CarMax has committed to dimming the parking lot lighting by approximately fifty (50) percent one hour after the close of business or 10:00pm, whichever occurs first. By comparison, the Duro-Life parking lot did not have such restrictions and was fully lit seven nights a week.

Landscape Plan

The proposed landscape plan shows a dense mix of shrubs and perennials around the perimeter of the parking lot areas, that will provide an attractive transition to the larger

green spaces as well as soften the look of the Corten steel guardrail system that CarMax requires around their sales display area for security purposes. Foundation plantings are also included where feasible around the front / west elevation of the building, as well as along the exterior of the six-foot tall masonry wall that is proposed around the vehicle staging area at the rear of the building. To mitigate the impact on the existing berming by the proposed re-grading of the site and pending road widening improvements, the petitioner has incorporated dense clusters of trees at the northeast corner of the site along Huntington Drive.

Development Agreement

CarMax has requested an incentive package from the Village in addition to their zoning approval to construct their facility. The package is structured as a sales tax rebate, and is therefore tied specifically to CarMax's performance. A draft Development Agreement is enclosed, which is under final review by the Village Attorney and CarMax, but the basic terms are as follows:

- The Village would rebate half (50%) of the Village portion of the sales taxes generated on site for a period of not more than ten years;
- The total amount shared with CarMax would not exceed \$800,000;
- In order to encourage an accelerated opening of the facility, if the facility is opened in 2020, the Village would share 75% the first year and 60% the second year, reverting to 50% in the subsequent years.

Because the agreement does not impact the zoning approval of the property, it was not reviewed by the Planning and Zoning Commission and is strictly under the purview of the Committee of the Whole and the Village Board. This agreement is similar in nature and form to two prior incentive agreements approved for Rosen Hyundai and Rosen's later expansion. As noted, this incentive is tied directly to CarMax constructing and operating its facility, and the performance thereof. There is no impact nor obligation upon existing taxpayers in Algonquin.

Planning and Zoning Recommendation

On July 9, 2018 the Planning and Zoning Commission considered the petition and unanimously recommended approval (6-0) of the request for Re-zoning from I-1, Industrial to B-2, Business, Final Plat of Subdivision, Final Planned Unit Development and a Special Use Permit for motor vehicle sales and, subject to the conditions listed by staff and an increased amount of landscaping along the property's frontage on both Randall Road and Huntington Drive.

Plan Revisions / Updates

The petitioner has worked to address many of the issues and concerns brought up at the public hearing, and below is brief summary of the revised plans that are included in this packet:

- Landscape Plan
 - Approximately 40 percent more trees have been added to the enclosed / revised plans, in particular at the northeast corner of the property along Huntington Drive. The landscaping along Randall Road has also been enhanced to include some landscape beds and more formalized grouping of trees to better frame the western side of property, as opposed to the more random placement of trees along Randall Road that were depicted on an earlier version of the Landscape Plan.
- Photometric / Lighting Plan
 - CarMax has agreed to reduce their proposed lighting levels by 33 percent, when compared to their original lighting plans that were presented to the Planning and Zoning Commission at the July 9 public hearing. Enclosed are the revised plans, which include colorized “heat maps” that illustrate a dark blue level approaching 0.0 foot candles at CarMax’s property lines, to demonstrate there will not be any light trespassing beyond the CarMax Lot 1 parcel. Additionally, the two colorized “heat maps” show through cooler light greens and blues, the significant reduction in light levels during overnight non-operational hours, when CarMax has committed to reducing the levels by 50 percent from operational hours (darker greens).
- Test Drive Route
 - CarMax has prepared two Test Drive Route Alternatives – exhibit enclosed – and Staff would recommend something similar to Option 1 which includes arterial and collector roads, such as Randall Road, Algonquin Road, Hanson Road and County Line Road. Conversely, there would be many more driveways / curb cuts along Bunker Hill Drive under Option 2 which involves driving through the residential neighborhoods south of Terrace Hill Golf Course.
- Masonry Wall Rendering
 - Enclosed is a color rendering illustrating the appearance of the 6-foot tall masonry wall enclosure that will screen the Staging parking area at the rear of the building. Per Staff’s recommendation, CarMax is indeed proposing the same brick and masonry block materials that will be used on the building itself.

Recommendation

Staff concurs with the Planning & Zoning Commission and recommends approval of the request for Re-zoning from I-1, Industrial to B-2, Business, Final Plat of Subdivision, Final Planned Unit Development and a Special Use Permit for motor vehicle sales and service with the following conditions:

1. The petitioner shall obtain a demolition permit from the Building Department and commence demolition of the existing building on the property within twelve (12) months of receiving final zoning and development approval by the Village.
2. The petitioner shall not remove any of the existing trees on the site – except for any trees that are dead or diseased – prior to obtaining a site development permit from the Public Works Department.
3. The Plat of Subdivision as prepared by Compass Surveying LTD with a latest revision date of May 3, 2018 shall be revised to incorporate comments from the July 3, 2018 Christopher Burke memo, the July 10, 2018 Public Works memo, and July 6, 2018 Teska Associates memo. A dedication or easement shall be added for the bike path along the East property line on Lot 3.
4. The Site Plan as prepared by Kimley Horn with a latest revision date of June 21, 2018 shall be revised to incorporate comments from the July 3, 2018 Christopher Burke memo, the July 10, 2018 Public Works memo, the July 6, 2018 Teska Associates memo, and May 30, 2018 Police Department memo. The trash enclosures shall match the design of the building, consist of a masonry exterior, and include space for recycling containers.
5. The Engineering Plans as prepared by Kimley Horn with a latest revision date of June 21, 2018 shall be revised to incorporate comments from the July 3, 2018 Christopher Burke memo, the July 10, 2018 Public Works memo, the July 6, 2018 Teska Associates memo, and May 30, 2018 Police Department memo. The petitioner shall consider revisions to the final design of the stormwater detention pond subject pending additional review by the Village Engineer.
6. The Photometric Plans as prepared by LSI with a latest revision date of July 18, 2018 shall be revised to incorporate comments from the July 3, 2018 Christopher Burke memo, the July 10, 2018 Public Works memo, and the July 6 Teska Associates memo. The pole-mounted light fixtures shall comply with village standards, and consist of flat black painted posts, downcast lights, have bulbs flush with housing on 25-foot poles. Light fixtures shall not have a tilt. All wall-mounted lights on the building shall be shielded and downcast with the housing covering the lumens. The Village Board shall have the ability to make additional modifications to the site lighting if, upon an inspection of the lights after installation, it is determined that the lighting is too intense.
7. The Landscape Plans as prepared by Kimley Horn with a latest revision date of June 21, 2018 shall be revised to incorporate comments from the July 3, 2018 Christopher Burke memo, the July 10, 2018 Public Works memo, and the July 6, 2018 Teska Associates memo. The area designated as Future Parking Held in Reserve shall include perimeter landscaping of shrubs and perennials, similar to elsewhere on site, if and when that area is developed for additional parking.

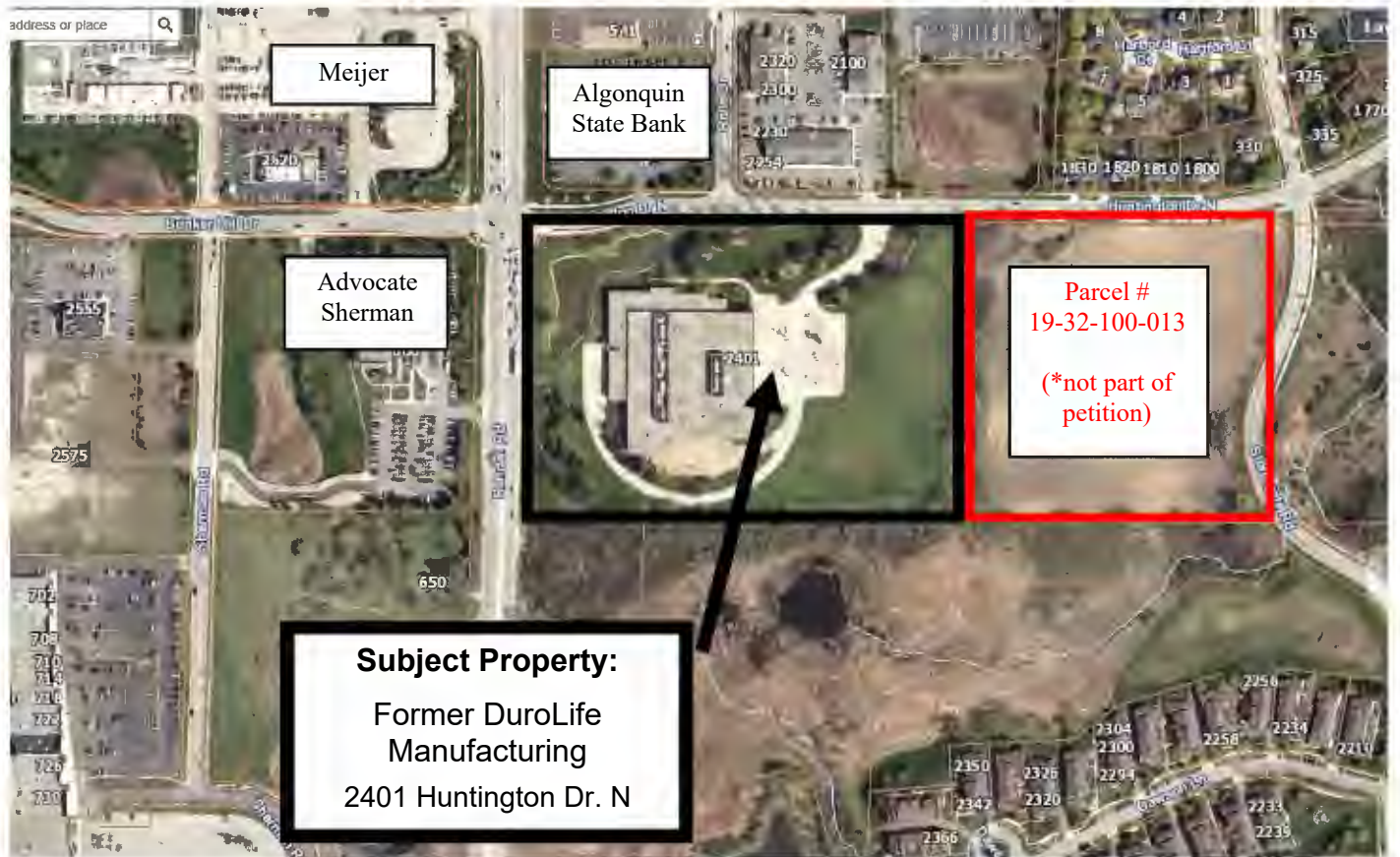
8. The Building Elevations as prepared by Charles J. O'Brien Architect with a latest revision date of May 3, 2018 shall be revised to incorporate comments from the July 3, 2018 Christopher Burke memo, the July 10, 2018 Public Works memo, and the July 6, 2018 Teska Associates memo. The brick and mortar may not be painted at any point in the future. All roof mounted or ground located mechanical/electrical equipment shall be fully screened with an appropriate architectural element or landscaping.
9. The Signage Plans as prepared by AGI with a latest revision date of May 1, 2018 shall be revised to incorporate comments from the July 3, 2018 Christopher Burke memo, the July 10, 2018 Public Works memo, and the July 6, 2018 Teska Associates memo. A maximum of one monument sign shall be permitted for the Lot 1 CarMax site. The entire perimeter and base of the monument sign shall be masonry to match the brick material on the building and a decorative stone cap shall be added to the top of the sign. The monument sign shall comply with the Village's Sign Code regulations, which include a maximum height of 10 feet from grade and surface area maximum for signable copy of 128 square feet if double-faced.
10. As an alternative to a second monument sign, the petitioner shall have the opportunity to install smaller secondary entrance signage, up to four (4) foot by four (4) foot in size, at the two access locations along Huntington Drive.
11. The wall signs proposed shall be revised to reduce the size of the "Service" informational sign so that is clearly accessory to the two CarMax business wall signs.
12. The petitioner shall submit revised sign drawings for review by the Community Development Department prior to issuance of any sign permits.
13. Lot 2 shall be required to come back in for review by the Planning and Zoning Commission and Village Board for Final PUD approval at the time plans are submitted to develop the future commercial site. A traffic study and stormwater report shall be required to be submitted by the developer of Lot 2, specific to their proposed commercial use of the site. The following uses shall be considered permitted uses as part of the B-2 zoning for Lot 2: hotels and motels; libraries; motion picture theaters; pharmacy with or without a drive through facility; schools (public or private). The list of Special Uses that shall be considered by the Village for Lot 2 will be limited to the following: automotive repair; auto parts and tire sales; child daycare services; carryout restaurants with drive through facility; packaged liquor store; car wash open to the general public; gasoline station with or without mini-mart sales.
14. Full illumination of the buildings and parking areas shall be limited to one hour after the close of business, or 10:00pm, whichever occurs first.
15. Automobile display area is limited to paved parking surfaces only, and no vehicle shall be placed on an elevated rack nor displayed with an open hood, doors, or trunk lid. No sales vehicles shall be allowed for display in the customer and employee parking lot.

16. CarMax shall advise its customers and employees that test driving and mechanics' drive patterns shall not occur on Huntington Drive east of the subject property. CarMax shall have their General Manager of the Algonquin location prepare a test drive map exhibit – substantially similar to Option 1 as depicted on their Draft Map date stamped received July 20, 2018 – to be approved by the Village and, after approval, to be handed out to its customers. The site exit to Huntington Drive shall be signed to indicate that test drives and vehicle carriers shall turn left only.
17. The following signs are prohibited: Inflatable signs, flags, pennants, banners, any sign with flashing or moving lights, search lights, and signs or stringers that move, excluding “Grand Opening” and “Now Hiring” banners which shall comply with the Village’s temporary sign requirements.
18. Reasonable effort shall be made to keep service doors closed at all times except when a vehicle is entering or exiting the building, or when determined by CarMax management that the doors should remain open for extraordinary reasons.
19. No storage of tires, parts, fluids or any materials, goods or waste products of any kind is allowed outside of an approved storage structure or building, and no goods or materials shall be stored higher than the walls of any outside storage area. All automobile fluids, including oil, grease and antifreeze must be stored and disposed of in accordance with all applicable regulations.
20. No inoperable or disassembled vehicle may be stored in the customer parking lot or sales area at any time and all work conducted on vehicles must occur within the service center.
21. The business hours for the service center shall be no earlier than 7:30am, and no later than 9:00pm.
22. The dealership shall educate its employees to refrain from using vehicle horns except in an emergency. Exterior loud speakers / paging systems shall be prohibited except for security / emergency purposes.

Enclosures

Subject Property Map
Staff / Consultant Memos
Randall Road Underpass Exhibit
Petitioner Narrative and Plan Submittal

Property in Question Map



**VILLAGE OF ALGONQUIN
PLANNING AND ZONING COMMISSION
Meeting Minutes
William J. Ganek Municipal Center-Board Room
July 9, 2018
7:30 p.m.**

AGENDA ITEM 1: Roll Call to Establish a Quorum
Present: Chairperson Patrician, Commissioners Hoferle, Szpekowski, Postelnick, Laipert, and Sturznickel.

Absent: Neuhalphen

Staff Members Present: Russ Farnum, Community Development Director, Ben Mason, Senior Planner, and Kelly Cahill, Village Attorney.

AGENDA ITEM 2: Approval of Minutes from the May 14, 2018 Meeting.
A motion by Commissioner Szpekowski to approve the May 14, 2018 minutes as presented was seconded by Commissioner Hoferle and a voice vote noted all ayes. The motion carried.

AGENDA ITEM 3: Public Comment
There was no one wishing to make any public comment.

AGENDA ITEM 4: Request for Rezoning, Final Plat Final PUD and Special Use Permit
Case No. 2018-09 CarMax
Petitioner: Greg Saia, Centerpoint

OPEN PUBLIC HEARING AND ESTABLISH QUORUM

Mr. Farnum called roll to verify a quorum. Present: Commissioners Hoferle, Szpekowski, Postelnick, Laipert, Sturznickel and Chairperson Patrician. Absent: Neuhalphen. Farnum announced a quorum was present. Chairperson Patrician opened the public hearing and asked for petitioner comments.

PETITIONER COMMENTS

Ms. Cahill verified that proper notice of the meeting had been posted. Ms. Cahill swore in the petitioners. Representing the petitioner was Peter Bazos, Attorney, J-M Dixon and Brian Maslyk, CarMax, Greg Saia and John Thatcher, Centerpoint Solutions, Michi Schuering and Daniel Grove, Kimley Horn. Mr. Bazos gave an overview of the property CarMax is proposing to redevelop at 2401 Huntington Drive North. The site was formerly occupied by Duro-Life Manufacturing and CarMax will subdivide into three lots. The property will also be downzoned from I-1, Industrial to B-2, Business, consistent with the surrounding commercial developments along the Randall Road corridor. Mr. Maslyk gave a summary of the CarMax company, which has store locations in over 40 states. CarMax has 9 stores in the Chicago area, that function in a hub and spoke system with larger production facilities and smaller satellite branches. The dealership facility proposed in Algonquin would serve as a satellite branch and consist of an approximately 7,000 square foot building footprint on a 5.25 acre site. By comparison, CarMax's Schaumburg location has an 87,000 square foot building on 22.5 acres and the Naperville location has a 60,000 square foot building on 13 acres.

Chairperson Patrician then asked for Staff Comments.

STAFF COMMENTS

Mr. Mason reviewed his staff report for the Commission. The petition is for Re-zoning the property to B-2, Business, Final Plat to subdivide the property into three parcels, Final PUD for CarMax Lot 1 and Stormwater Detention Lot 3, and Special Use Permit for motor vehicle sales and service. Lot 2 will be required to come back before the Planning and Zoning Commission and Village Board for Final PUD in the future. Access to the site will remain off Huntington Drive and McHenry County DOT has indicated it would allow a right-in / right-out off Randall Road at the south end of the property when Lot 2 develops. The majority of vehicle trips will be from Randall Road and overall traffic flow in the area will be improved as a result of McHenry County DOT's planned widening of the Randall / Huntington intersection. CarMax is willing to work with the county and deed the land necessary at the north end of the subject property for the intersection and road improvements. The building will have a brick exterior on all four sides and CarMax Lot 1 is allowed one ground sign. Staff is recommending several conditions on the dealership's operations, including lighting, noise, test drives, signage, and hours of operation. Staff recommends approval of the Re-zoning, Final Plat, Final PUD and Special Use Permit consistent with the findings of fact and the conditions listed in the July 9, 2018 Community Development staff report. Ms. Cahill read aloud the findings of fact from the July 6, 2018 Teska Associates memo.

COMMISSION QUESTIONS/COMMENTS

Chairperson Patrician inquired if there were any Commissioner questions or comments.

Commissioner Hoferle asked about the potential for adding landscape islands in the Sales parking lot, to which Mr. Mason stated staff is comfortable with only requiring islands in the Customer and Employee lot as landscaping, trees, birds and other wildlife can cause a nuisance directly adjacent to a dealership's sales inventory. Commissioner Hoferle suggested CarMax add more landscaping along the property's Randall Road frontage.

Commissioner Szpekowski asked for clarifications of the security gates around the Sales lot, to which Mr. Maslyk explained there will be both a 20-foot wide access gate to separate from the Customer and Employee lot, as well as a Corten guardrail around the entire perimeter of the Sales lot. Commissioner Szpekowski suggested CarMax provide a test drive route map for the Village Board to review.

Commissioner Postelnick asked about the timing of vehicle carrier deliveries, to which Mr. Maslyk stated it is typically done during business hours as a sales associate is required to be on site to receive the vehicles. Mr. Maslyk explained the service building will typically operate weekdays 7:30am-6:00pm and the sales building will likely have regular 9:00am-9:00pm hours Monday through Saturday. Commissioner Postelnick asked about lighting, to which Mr. Maslyk explained CarMax dims the parking lot fixtures by approximately 50 percent one hour after the close of business.

Commissioner Sturznickel noted the sales tax benefit of an automotive dealership to a community, to which Mr. Bazos explained CarMax doesn't have specific numbers projected for the Algonquin location at this time.

Commissioner Laipert asked about the height of the parking lot light fixtures, to which Mr. Maslyk indicated they are proposed to be 26 feet tall and LED fixtures.

Chairperson Patrician asked about light levels extending beyond the property, to which Mr. Mason explained CarMax will be required to meet village standards of downcast lights and no trespass light beyond their lot. Chairperson Patrician asked about noise from things such as blowers and air wrenches, to which Mr. Maslyk explained that CarMax keeps their service doors closed at all times, except when a vehicle is entering or exiting, and the cleaning of car interiors is done with a common ShopVac vacuum cleaner.

PUBLIC COMMENT

Chairperson Patrician called for public comments.

Rick Orbutt, 295 Winchester, expressed concern about the impact the CarMax development will have on wildlife and loss of trees on site, light pollution, and constant traffic in and out of the site. Mr. Orbutt stated it would be preferable if the site remained industrial and CarMax filled an empty storefront somewhere else in the community.

Julie Weingarz, 2200 Huntington, explained her office is located directly north of CarMax's proposed Customer and Employee parking lot and inquired about the potential for CarMax to plant new, mature trees along Huntington and expressed concern about the dealership's potential use of loud speakers.

Justin Roach, 1820 Huntington, expressed his house is directly east of the property and expressed concern about the impact the CarMax development will have on nearby residential property values and cited a realtor who explained to him his house value would depreciate by approximately 25 percent. He expressed concern about vehicle test drives using streets in residential areas and noted that birds were deemed to be a nuisance by CarMax.

Nick Tabeck, 7 Hartford, noted that the former Duro-Life manufacturing business on the property had been a very good neighbor and did not create any noise.

Glen Blum, 200 Salford, expressed concern over existing traffic congestion on Huntington Drive, the impact CarMax development would have on nature and trees on the former Duro-Life property, safety issues caused by vehicles entering and leaving the proposed CarMax business, and noise from loud speakers and the servicing of vehicles with equipment like air wrenches.

Nancy Blackman, 2062 Tunbridge, stated the location CarMax has chosen is not appropriate due to the availability of many other vacant retail stores such as the former Gander Mountain building, and expressed concern about lighting and traffic that will be generated by CarMax.

Bonnie Biondo, 2 Bedford, expressed concern about the impact to surrounding property values, lighting, and stated another location for CarMax would be preferred, such as the former Gander Mountain building or an old Dominick's.

Bob Smith, 1571 Glacier, expressed support for the proposed CarMax development and noted the former Duro-Life building has sat vacant for several years and noted the economic benefits of having the property revitalized, to which Ms. Dixon with CarMax cited that the company has not closed a location in its 25 years of being in business.

Tom Zanck, 40 Brink Street, Crystal Lake, stated that the current zoning of the property is I-1, Industrial and other alternative uses that would be allowed on the property would be more intensive than the proposed CarMax, such as a tool and die manufacturer or machine shop. He expressed support for CarMax and their proposed site plan.

Barbara Siepiora, 345 Aberdeen, expressed concern about the increase in traffic CarMax would generate and safety of students walking past the property on their way to and from Jacobs High School.

John Bridges, 3600 Lakeview, expressed concern about test drives in residential areas.

CLOSE PUBLIC COMMENT

Chairperson Patrician closed public comment.

Chairperson Patrician attempted to summarize the main concerns and issues that had been raised regarding landscaping, lighting and traffic congestion. Mr. Mason clarified that similar to all other developers, CarMax will be required to replace trees on site at a 1-to-1 ratio or pay the village's \$100 per caliper inch fee-in-lieu of tree replacement. Mr. Mason also stated CarMax is required to follow the village's standards for parking lot lighting, that all fixtures will be downcast and noted the more intense light levels proposed for the Sales lot along Randall Road will be reduced by 50 percent during non-operational hours. Mr. Mason explained that compared to other retail and commercial uses, the projected number of 200 vehicle trips generated by CarMax will be quite low and fortunately McHenry County DOT has included significant improvements to the Huntington Drive intersection as part of the Randall Road widening that will help alleviate much of the traffic congestion on Huntington and Rolls Drive that exists today.

Mr. Bazos noted that CarMax had a professional appraisal report done, which did not find any material change to neighboring property values located adjacent to several other automotive dealerships in the area, including Rosen Hyundai on Harnish Drive. He stated that CarMax is committed to producing and following a defined test drive route that meets village approval.

Commissioner Hoferle reiterated his recommendation that additional landscaping be incorporated along the perimeter of the site fronting onto both Randall Road and Huntington Drive.

COMMISSION MOTION ON PETITION

Chairperson Patrician entertained a motion to approve the request for re-zoning the subject property from I-1, Industrial to B-2, Business, Final Plat, Final PUD, and Special Use Permit for motor vehicle sales and service. Commissioner Sturznickel moved and Commissioner Szpekowski seconded a motion to recommend approval of the request consistent with the findings of fact listed in the July 9, 2018 Community Development memorandum, July 6 Teska Associates memorandum, the conditions recommended by staff, and a further recommendation that additional landscaping be added on the western perimeter of the property along Randall Road.

The Roll Call noted the following: Ayes: Commissioners Laipert, Szpekowski, Postelnick, Sturznickel, Hoferle and Chairperson Patrician. Nays: None. Absent: Neuhalfen. Motion carried 6-0.

AGENDA ITEM 5: New/Old Business

None

AGENDA ITEM 6: Adjournment

A motion to adjourn the meeting was seconded and a voice vote noted all ayes. The motion carried and the meeting was adjourned at 10:15p.m.

Respectfully Submitted,

Benjamin A. Mason, AICP
Senior Planner

DRAFT



To: Ben Mason, AICP

From: Mike Hoffman, AICP, PLA

Date: July 6, 2018

Subject: Case #2018-09 - CarMax Auto Superstore

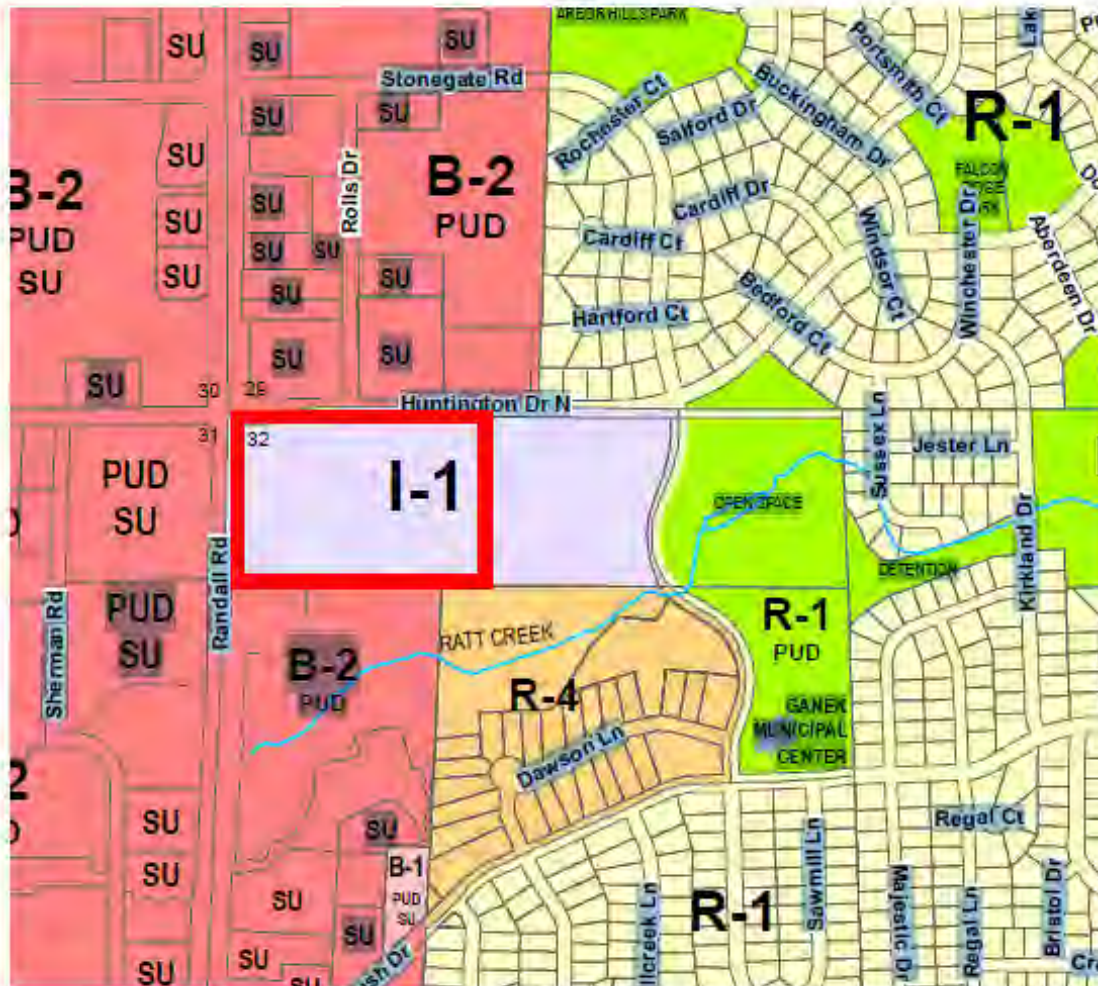
CarMax Auto Superstore would like to open a new facility in Algonquin at the southeast corner of Randall Road and Huntington Drive. This property was the former location of Duro-Life office and manufacturing facility. The property is currently zoned Industrial (I-1). The request is to rezone the property to B-2, with a Special Use Permit for Used Auto Sales. They have also requested a preliminary/final plat for a three-lot subdivision, and preliminary/final PUD for the CarMax and detention lot and Preliminary PUD for the remaining undeveloped lot. This memo updates out 6/4/2018 letter based on revised plans provided by the applicant.

The entire site contains 13.83 acres. This is proposed to include three lots – a 6.76-acre lot containing the CarMax, a 4.85-acre lot to the south for future development, and a 2.22-acre lot to the east for detention. The existing Duro-Life building and site improvements will be removed to allow for new construction of the CarMax facility.



Surrounding Use and Zoning

	North		South	East	West
Existing Land Use	Algonquin Bank, offices	State	Wetlands, Dawson Mills Park (open space)	Vacant lot	Advocate Outpatient Center
Existing Zoning	B-2 PUD		B-2 PUD (R-4 to the southeast)	I-1	B-2 PUD

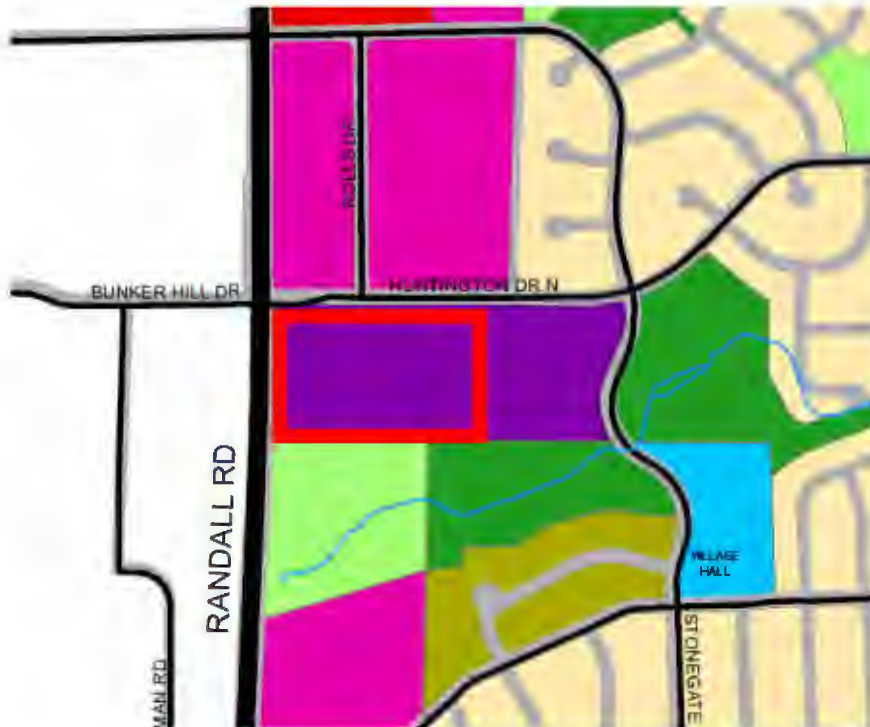


Zoning Map

From review of the surrounding land use patterns, and the existing zoning, the pattern of development in this area has been for commercial uses (predominately retail, office, and dining) consistent with the B-2 zoning category. The Algonquin zoning code allows for a car dealership (new or used) to be within an I-1 District as a special use. However, the rezoning to B-2 will improve consistency with surrounding zoning and will be consistent with how similar facilities like the nearby Rosen Hyundai has been zoned.

Relationship to the Comprehensive Plan

As illustrated below, the Future Land Use Map in the Algonquin Comprehensive Plan shows this site for business park/light industrial use. Property to the north and west is illustrated for Planned Mixed Use (predominately commercial), with land to the south planned for parks and open space given the extensive wetland complex in the area. A review of the map below clearly shows that the business park/light industrial classification for this property was based on the prior existing use, Duro-Life. In the absence of that use, it is likely that the property would have been planned for mixed use to be consistent with the treatment of other property both north and south along Randall Road.



Algonquin Future Land Use Plan

Zoning Standards

As can be seen in the following table, the plans comply with all zoning bulk requirements.

	Front Setback	Rear Setback	Side Setback	Building Height
B-2 Standard	30'	20'	0'	35'
Proposed	140'	50'	270'	28'

Site Plan

- A path is illustrated on the eastside of the site. This will provide a connection to the east-west trail that is planned to connect under Randall Road to Jacobs High School. That east-west connection will be south of the subject property. However, the connection shown on this plan

will provide an important connection north to the shopping and dining options along Randall Road.

Lighting

The lighting plan is acceptable as proposed. LED lighting will be used, and fixtures are all appropriately directed down to minimize glare. Light levels range from 6 to 13 foot-candles in the sales lot which is high for a normal parking lot, but within a reasonable range for an auto sales lot. There are no residential properties in the immediate vicinity of the proposed use. Light levels in the customer/employee lot are lower, in the 2 to 6 foot-candle range. While light levels appear low at the main (eastern) entrance at Huntington Drive, there is an existing street light at this intersection which should provide appropriate additional lighting.

Architecture

The building will be primarily four-sided brick, with split-faced concrete block at the base. Horizontal color banding has been used to provide some visual relief. Canopy entrances are planned on both the Randall Road and Huntington Drive sides of the building. E.I.F.S. is used on the columns for the canopy, which highlights the entrances and provides some visual relief.

Landscape

- Plant selection is good, with nice variety of trees and shrubs.
- Plant schedule:
 - A dry-bottom detention mix, which appears to be proposed on the side slopes of the detention area, is noted in the schedule. Mix details should be provided to verify appropriate species.
 - Plugs are noted for planting in the north end of the pond. The type of plugs (species, size, spacing) should be provided.
 - Planting size should be specified (3" caliper trees, shrub size, etc.)
- Planting for the south end of the detention should be specified.

PUD Standards

The Algonquin Zoning Ordinance outlines the following standards for planned developments (listed in italic, with our comments on how these standards apply to this case below):

1. *The number of dwelling units erected shall not exceed the number permitted by the regulations of the district in which it is located, except in cluster developments where a density bonus may be granted as part of the planned development.* – not applicable
2. *If a building is permitted to exceed the height limit of the district in which it is located, the yards and open spaces around such building shall be increased by an amount equal to the height that the building exceeds the height limit of the district measured in feet.* – Not applicable, as no height increase requested.
3. *If more intensive uses are permitted than are allowed by the district regulations, there must be clear evidence that such uses are appropriate, provided the Planning and Zoning Commission finds:*
 - a. *That the use permitted by such exceptions is necessary or desirable and is appropriate with respect to the primary purpose of the development;*
 - b. *That the uses permitted by such exception are not of such a nature or so located as to exercise a detrimental influence on the development nor on the surrounding neighborhood;*
 - c. *That, in an industrial development, such additional uses allowed by exception shall conform to the performance standards of the district in which the development is located as set forth in Section 21.4 herein;*
 - d. *That the use exceptions allowed are on file in the Community Development Department;*
-
The proposed use is not more intense than would otherwise be permitted within the B-2 District.
4. *The amount of off-street parking must be adequate to serve the needs of the projects. The Planning and Zoning Commission and Village Board may require more or less off-street parking, than is otherwise required by this Chapter if it is determined the use(s) warrants the deviation.* The Algonquin code does not have a specific parking standard for automobile dealerships. Most of the parking proposed will be for the sales lot, with customer and employee parking provided in separate lots along Huntington Drive. From our review, it appears there is adequate space provided for employee and customer parking. In addition, adequate space is available to expand this parking should the need arise in the future.
5. *If any open space or recreational facility is to be used solely by the residents of the project, adequate provisions shall be made for assessments against the property within the project so that such facilities can be properly improved, maintained, and operated.* – Not applicable.
6. *All residential planned developments that involve annexation shall include clearly identifiable community-wide benefit improvements to the Village. Benefits may include the following, but shall not be limited to those listed: expansion of Village infrastructure that can serve other parts*

of the community; creation of a community park; dedication of right-of-way or construction of a collector road; and component of a larger mixed-use development that includes commercial uses. The larger the residential unit count, the larger the community-wide benefit improvements. – Not applicable.

7. *All commercial planned developments that include lots fronting on any major collector or arterial roadway shall provide landscaped open space between each building and parking lot. Said landscaped area shall include combinations of trees, shrubs, and seasonal plantings that shall be planted on top of earthen berms. The appropriate land area, height of the berm, and exact plant material specifications shall be reviewed and approved by the Village Board as part of the final planned development review process.* – The proposed CarMax provides landscape space around all sides of the development (see earlier comments on suggested landscape plan improvements).

Rezoning Standards

The Algonquin zoning code does not list specific criteria for review of a zoning amendment. Where such criteria do not exist, the standard in Illinois is to rely on what are commonly referred to as the “LaSalle Factors” based on an Illinois Supreme Court ruling on a zoning case LaSalle National Bank of Chicago v. County of Cook. Courts often also consider an additional two factors (7 and 8 below) from the Sinclair Pipeline Company vs. Village of Richton Park. These factors are combined below and noted in italics, along with our opinion as to how this specific zoning request complies with these criteria.

1. ***The existing uses and zoning of nearby property*** - As noted above, the predominate zoning along Randall Road is B-2 PUD, consistent with this request. Surrounding uses are all commercial in nature. The existing I-1 zoning is not consistent with surrounding properties.
2. ***The extent to which property values are diminished by the particular zoning restrictions.*** The applicant should provide testimony to this point, but it is likely that if another industrial user had interest in reuse of the facility that would have occurred by now. In contrast, there is a clear pattern of successful commercial development with B-2 Zoning in the Randall Road corridor.
3. ***The extent to which the destruction of property values of plaintiff promotes the health, safety, morals or general welfare of the public.*** Development of a CarMax on this property will provide Algonquin residents with a convenient location to purchase a car and will provide additional sales tax revenue. The additional trail connections provided around the property will enhance the health and safety of residents.
4. ***The relative gain to the public as compared to the hardship imposed upon the individual property owner.*** The property owner could maintain the existing I-1 zoning and just request a special use for the motor vehicle sales. However, it is in the interest of both the Village and the property owner to enhance the consistency of the zoning in the area by rezoning the property to B-2.
5. ***The suitability of the subject property for the zoned purposes.*** Given the location at a major intersection along Randall Road, it is better suited for a commercial use (B-2 zoning) when

compared with the existing industrial zoning.

6. ***The length of time the property has been vacant as zoned considered in the context of land development in the area in the vicinity of the subject property.*** The property has sat vacant for several years. The Randall Road corridor remains a strong commercial corridor. The existing vacant industrial use was constructed prior to the commercial development boom in the corridor. Industrial uses like this are more appropriate in larger business parks.
7. ***The community need for the proposed use.*** Reuse of this site for a commercial activity will provide residents with a convenient location to purchase a car and will provide new jobs for the community.
8. ***The care with which the community has undertaken to plan its land use development.*** The Village's Comprehensive Plan shows business park/industrial use for this property based on the prior use. However, the land use pattern along Randall Road is clearly focused on commercial uses, and the proposed rezoning would be consistent with the surrounding development patterns and the economic development goals of the Village.

Recommendation

Subject to additional concerns raised at the public hearing, we recommend approval of the requested rezoning to B-2, approval of the use of the site for used car sales, preliminary and final plat approval, and preliminary PUD approval for all three lots and Final PUD approval for the CarMax and detention lots, subject to modifications underlined above and the recommendations contained in the Traffic Study prepared by Kimley Horn and dated May, 2018.

In addition, we would recommend a future modification to the Village's Comprehensive Plan to change the land use designation for this property to Planned Mixed Use (predominately commercial) and the property to the immediate east to Planned Mixed Use (predominately residential). Once completed, we would also recommend rezoning of the property to the east from I-2 to a more appropriate residential category – likely R-4.



Algonquin-Lake in the Hills Fire Protection District

1020 West Algonquin Road • Lake in the Hills, IL 60156 • (847) 658-8233 • Fax: (847) 854-2609

To: Ben Mason

From: Michael D. Murphy

Subject: Case No. 2018-09
CarMax

Date: July 2, 2018

Ben the Fire District has no issues with this proposal.

Trustees
Rick Naatz
John Bradach
Michael Markowitz
Charles Teson
Bruce Tousaint

Fire Chief
Peter Van Dorpe

Commissioners
Jeff Harper
Tamara Miner-Corso
Timothy Moss



Village Of Algonquin Police Department

~MEMORANDUM~



DATE May 30, 2018
TO Ben Mason, Senior Planner
FROM Sergeant Robert Salazar
SUBJECT Case No. 2018-09 CarMax

The plans for CarMax and there are no objections to the plans submitted to the police department. Confirming that the western entrance for the parking lot off of Huntington Dr. will be ingress only for E/B traffic, this entrance should be clearly marked not only on Huntington Dr. but also by "do not enter" signage in the parking lot.

As a reminder, the handicap parking fine should be displayed as \$500 and not \$100 to conform with the Village of Algonquin parking ordinance.

[Email](#)

[Print Form](#)



CHRISTOPHER B. BURKE ENGINEERING, LTD.

9575 West Higgins Road Suite 600 Rosemont, Illinois 60018 TEL (847) 823-0500 FAX (847) 823-0520

July 3, 2018

Village of Algonquin
2200 Harnish Drive
Algonquin, IL 60102

Attention: Ben Mason, Senior Planner

Subject: CarMax – Rezoning, Final Plat, Final PUD, SUP - SECOND REVIEW
Algonquin Case No. 2018-09
(CBBEL Project No. 07-0272.00107)

Dear Ben:

We have reviewed the following documents related to this project:

- Preliminary Engineering Plans prepared by Kimley-Horn bearing a revision date of June 21, 2018
- Landscape Plans prepared by Kimley-Horn bearing a revision date of June 21, 2018
- Final Plat of Subdivision prepared by Compass Surveying, Ltd. bearing a revision date of May 3, 2018 (**INCORRECT REVISION DATE**)
- Preliminary Drainage Report, prepared by Kimley-Horn and Associates, Inc., dated June 22, 2018
- Lighting Plans prepared by LSI bearing a revision date of May 7, 2018 **PREVIOUSLY SUBMITTED**
- Geotechnical Engineering Services Report prepared by Professional Services Industries, Inc. bearing a revision date of February 14, 2018 **PREVIOUSLY SUBMITTED**
- EcoCAT report dated May 2, 2018 **PREVIOUSLY SUBMITTED**
- ALTA/NSPS Land Title Survey prepared by Compass Surveying, Ltd. bearing a revision date of February 7, 2018 **PREVIOUSLY SUBMITTED**

It is our understanding that this project will be presented to the Planning and Zoning Commission on July 6, 2018. We offer the following comments for their consideration:

PRELIMINARY ENGINEERING PLANS

Sheet C1.0

1. The striping for the full (east) access driveway is missing from the plan sheet; one inbound land, one outbound left turn lane, one outbound shared thru and right turn lane. It appears from the engineer's response that he misinterpreted this previous comment.

2. Sidewalk crossing striping should be added across the driveway where the connecting sidewalk from Huntington Drive intersects with the internal driveway. It appears from the engineer's response that he misinterpreted this previous comment. We are referring to an on-site location adjacent to the pop-up security gate separating the outside vehicle storage from the customer parking area.
3. The section of newly added mountable curbing along the south side of drive aisle (adjacent to the carrier stripe) should be revised to a barrier curb & gutter section to provide more positive drainage to the inlet structure to the east.
4. A bike path paralleling the south property line will be constructed prior to the construction of the proposed CarMax facility. The design engineer has been provided with the plan for these improvements and should include that information in the final engineering plans so that the connection with the bike path on the CarMax site can be properly coordinated. The engineer may have to deflect the alignment of the pathway around the south end of the detention pond to connect to the west side of the bike path boardwalk abutment or construct a portion of additional boardwalk to make a perpendicular connection (typical to all applicable plan sheets). In addition, the shape of the proposed detention pond should be revised to provide a more curvilinear shape, at least along the east side, to allow for the bike path to also have a curvilinear alignment that will allow for more elaborate clumped plantings and visual interest.
5. The existing crossing striping on Huntington Avenue adjacent to the proposed east entrance is missing from the plan sheet. This striping will need to be extended and/or revised to reflect the new geometry of the entrance and connection with the proposed bike path on the CarMax site (typical to all applicable plan sheets).
6. It appears that the plan scale is to be 1" = 50', but the graphic scale on the plan sheet does not correspond to this (typical to all applicable plan sheets).

Sheet C2.0

7. The engineer shall verify the spot elevations of ME 907.70 and ME 898.94 along the Huntington Drive right-of-way line. The two spot elevations are not consistent with the proposed contours and other adjacent proposed spot elevations.
8. In conjunction with comment 3 above, the proposed grading and contours along the southern side of the drive aisle will need to be revised to reflect the change to barrier curb & gutter.
9. The engineer states in his response letter that a swale has been proposed at the top of the berm along the south property line to convey the surface flow to the proposed detention pond, but due to the scale of the drawing and the tightness of the contours, this cannot be verified now. Cross-sections of the swale should be provided to verify that the swale has adequate capacity to trap and convey the run off to the basin. The

final engineering submittal should provide a plan view of this area at a larger scale to provide clarity of the design intent.

10. In conjunction with the above, the elevations of the swale and the rim of structure D12 (which the swale drains to) are lower than the elevation of the HWL of the detention pond. Water would back up for 2/3 of the length of the swale during 100-year conditions.
11. The location and grading of the overflow weir for the detention pond is not depicted on the plan sheet.
12. The proposed rim of the outlet control structure (D15) is 0.38 feet lower than the detention pond HWL elevation. The pond will overflow through the rim as designed before it reaches the noted HWL. The rim will have to be raised to provide the required pond volume. The placement of the outlet control structure should be out of the flow path from the overland overflow weir.
13. The proposed bike path will be running along the east side of the detention pond berm so a swale should be constructed between the edge of path and the toe of the slope to prevent flooding of the pathway. It appears that this is the case with the proposed design, but again is difficult to verify given the scale of the plan sheet. Cross sections at 50' spacing are required along the length of the path to confirm that proper slope and drainage is provided within the final engineering plan set.

Sheet C3.0

14. The existing conditions information fails to show the existing storm sewer outfall from the present site to the wetland to the south. The outlet pipe for the proposed basin should discharge at the same location to mimic the existing hydrology and to discharge under the boardwalk for the proposed off-site bike path. This information was provided to the engineer and would affect the discharge location of Storm structure D16.
15. The callout for structure EX-S1 in the SANITARY STRUCTURE TABLE is not consistent with the plan note calling for a wye connection and the existing conditions information. The engineer shall verify if an existing manhole exists at this location, or if not, then a new one shall be constructed on top of the existing main.
16. As depicted in the plan view and determined by the noted invert elevations in the SANITARY STRUCTURE TABLE, the sanitary sewer between structures S4 and S10 is back pitched.
17. The information for storm structures D4, D5, and D8 are missing from the STORM STRUCTURE TABLE. Structures D4 and D5 will need to 5' in diameter or larger to accommodate the large diameter storm sewers that they service.

18. The engineer shall verify the pipe slopes between storm structures D8-D9 and D9-D5 as they appear that they should be 3.07% to match the given upstream and downstream inverts.
19. The engineer shall verify the information listed for EX-FES as it appears that a proposed storm sewer extension with attached structure is depicted on the plan view for this location.
20. The invert for structure D16 should be noted as "out" versus "in".
21. Storm structures D5 and D10 should be specified to be catch basins versus manholes.
22. There is a plan note for an 18-inch storm sewer stub to serve the Surplus Lot that is not depicted on the plan sheet or listed in the STORM STRUCTURE TABLE.
23. The connections of the proposed watermain loop to the existing Huntington Drive watermain shall be specified as a pressure-connections with a valve in vault at the point of connection; the proposed valve and vault on the south side of Huntington Drive can be eliminated. The use of a pressure connection is subject to approval by Village staff upon the verification of the diameter of the existing watermain.
24. The plan view shows a double line for the buildings water service which is different from the single line in the sheet legend. The engineer shall note that there is to be only one common fire-domestic water service line to serve the building; the domestic service line will be split from the common line inside the building.
25. The boundaries of the noted utility easements will need to be revised to encompass the proposed fire hydrants (typical to this plan sheet and Plat of Subdivision).
26. The pavement removal and replacement associated with the installation of the proposed water and sanitary service lines shall be depicted on the plan sheet with the final engineering submittal.
27. The existing water and sanitary sewer service lines for the building to be demolished are not depicted on the plan sheet. These service lines are to be capped off at the respective mains if they are not to be reused. The plans shall callout the removal items as well as the replacement of any disturbed pavement. We acknowledge the engineer's response that a demolition sheet shall be added to the final engineering submittal depicting this work.
28. The slope and inverts for the proposed 6-inch roof drain is missing from the plan sheet. We acknowledge the engineer's response that this information will be included with the final engineering submittal.

29. The utility crossing information is missing from the plan sheet. We acknowledge the engineer's response that this information will be included with the final engineering submittal.

Sheet C4.5

30. The following standard details shall be included with the final engineering submittal:
- Storm inlet
 - Storm catch basin
 - All applicable SESC devices

PHOTOMETRIC PLANS

31. There remains one lighting unit along the southern property line that conflicts with a proposed tree. Either this plan or the Landscape plan will need to be revised accordingly.
32. Provide project specific catalog cuts for the Proposed light standards and luminaires. These cut sheets shall include overall mounting heights and specific luminaire details. We acknowledge the response that this will be included with the final engineering submittal.
33. The plans should show proposed concrete foundation details in the plans including the height of the foundation above grade for each pole configuration. We acknowledge the response that this will be included with the final engineering submittal.

LANDSCAPE PLANS

34. The proposed light pole along the south side of the customer parking at the southeast corner of the lot is missing from the plan sheet.
35. There remains one lighting unit along the southern property line that conflicts with a proposed tree. Either this plan or the Photometric plan will need to be revised accordingly.

FINAL PLAT OF SUBDIVISION

36. The legal description for Exception 1 lists an arc length of 88.34 along a curve with a radius of 265' along the southern right-of-way line for Huntington Drive. This same line segment has a noted deeded length of 99.34' on sheet 1 (plan view). The surveyor shall verify the dimension and revise the document accordingly.
37. The correct revision date should be placed in the revision block on both sheets.

38. The boundaries of the proposed utility easements shall be revised so that the proposed fire hydrants are encompassed by the easements (typical to Plat and Preliminary Engineering plan sheet C3.0).
39. Revise the label of "Hereby dedicated to the Village of Algonquin" to "Hereby dedicated to the Village of Algonquin for Roadway & Utility purposes" on sheet 1.
40. Revise the label of "(Detention Area)" on Lot 3 to "(Detention Area and Drainage Easement)" unless the lot is to be turned over to the Village for ownership and maintenance.
41. Drainage\Detention Easement Provisions for Lot 3 should be added to sheet 2 if the lot will not be turned over to the Village for ownership and maintenance.

STORMWATER COMMENTS

42. The Engineer's response noted that while the detention basin has been designed for the overall acreage of the entire 13.5-acre parcel, it has not been designed to include an increase in imperviousness attributed to future development on the southern parcel. Additional detention will be required if/when the southern parcel develops. It is still unclear if the storm sewer system has been designed to include future imperviousness from the southern site. Since it appears stubs will be provided to the southern parcel, it would be necessary to account for future imperviousness of this parcel to provide adequate capacity to the detention basin.
43. The engineer shall provide overland flow calculations during final engineering that demonstrate runoff from the western portion of the site can access the detention basin during overflow events.
44. The final engineering submittal shall include storm sewer and swale capacity calculations.
45. It appears the detention basin is a wet-bottom pond based on invert and bottom pond elevations. Therefore, the area that will maintain a normal water level should be included as impervious in the Stormwater detention volume calculations. Additionally, volume below the restrictor invert elevation should not be included in the detention modeling. This volume is considered dead-storage.

OUTSIDE PERMITTING AGENCIES

46. A permit will be required from the IEPA for the proposed watermain extension. The engineer shall prepare and submit draft application documents with his submittal for final engineering.
47. A permit will be required from the IEPA for the proposed sanitary sewer extensions. The engineer shall prepare and submit draft application documents with his submittal for final engineering.

48. A permit will be required from the IEPA for the site disturbance associated with this project. The engineer shall provide a copy of the document with the final engineering submittal or provide the permit number to the Village.

GENERAL COMMENTS

49. The applicant shall provide proof that the TIS was submitted to the McHenry County Division of Transportation, all subsequent correspondence, and its approval letter when received.
50. The final engineering submittal shall include all applicable SESC details and specifications and SWPPP.
51. The final engineering submittal shall include all applicable pavement and utility standard details and specifications.

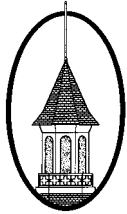
Sincerely,



Paul R. Bourke, PE CFM CPSM
Assistant Head, Municipal Department



Michael E. Kerr
Vice President, Civil Department



VILLAGE OF ALGONQUIN

PUBLIC WORKS DEPARTMENT

– M E M O R A N D U M –

DATE: *Tuesday, July 10, 2018*
TO: *Ben Mason*
FROM: *Shawn M. Hurtig*
SUBJECT: *Public Works Review # 2*
CarMax (CD2018-09)

Please find below the Village of Algonquin comments, concerns, and issues regarding the subject project.

Permit Requirement Status:

1. Provide a report outlining the likely site development permits required for this project (Examples: IEPA Water, IEPA Sanitary, IEPA NOI, ACOE, etc....)

Project Document Status:

1. Ph. 1 Environmental Report (Preliminary Site Assessment – PSI) = Submitted & Dated 2-14-18 (no comments)
2. Ecological Reports = Submitted & Dated 5-2-18 (no comments)
3. Geotechnical Report = Submitted & Dated 2-14-18
 - a. Please note that Village minimum standards shall be met for pavement and base aggregate thickness
 - b. Please note that Village minimum standards shall be met for trench sections
 - c. Please note that Village must be copied on all subgrade stabilization mix designs (should that corrective measure be requested by developer)
4. Stormwater Report
 - a. Submitted & Dated 5-7-18 (no comments)
 - b. Submitted & Dated 6-22-18
 - i. Please note that should the identified wetland be filled, a mitigation of this existing condition shall be conducted
 - ii. Please note that should the identified wetland buffer be filled, a mitigation of this existing condition shall be conducted
5. Traffic Analysis = Submitted & Dated May 2018
 - a. Future conditions do not take into account the remaining outlot (only is considering CarMax lot) and the traffic it produces.
 - i. In the Turn Lane Warrant it lists that a LTL is warranted on Huntington should there be a vol of 15 or more left runs generated. Currently the CarMax only indicates 10 on the West entrance (B) and 5 on the East entrance (A), however as noted above this volume does not include the outlot traffic generation.
6. Water Customer Data Sheet = Not Submitted

Plat Review Comments:

Final Plat of Subdivision of CarMax 6152 dated 5-3-18

1. Plat shall include all easements.
 - a. Indicate easement for bike path along East property line

Plan Review Comments:

PAGE

ISSUE

Misc.	Demolition and Removal plan sheet(s) is missing. That plan shall indicate the removal of the existing building, parking lot, curb/gutter, new curb cut removals, old curb cut removals and barrier curb replacement, storm sewer removal, water service removal, sanitary service removal, etc... Tree removal shall be on a space plan page
Misc.	Soil Erosion and Sediment Control (SESC) plan sheets missing. That plan shall indicate all the temporary and permanent SESC measures such as silt fence, construction entrance, inlet baskets, ditch checks, temp seeding, stock pile protection, and all associated SESC inspection and maintenance notes.
C1.0	The geometry of the roadwork on Huntington is not consistent with the last set of plans received by the Village by McHenry County DOT for the Randall Road Widening project. It should be noted that the Village is requesting clarification from the County on any revisions made to this roadway post bid set and those revisions will be subject to review and revision by the Village of Algonquin.
C1.0	Plans shall ensure proper alignment of bike path crossing of Huntington Drive. This path will also be crossing a collector roadway midblock without stop control, as such a rapid round flashing beacon assembly shall be installed
C2.0	There is a swale indicated along the south property line, however the grading plan does not reflect the depth nor is there a cross section depiction of this swale. In addition, the spot grades shown in the swale do not make sense
C3.0	List the structure sizes and frame types for each installation. Follow Village iron requirements for all Village owned assets
C3.0	Provide unique identifiers for each structure, including hydrants
C3.0	The proposed water loop is in an appropriate location however the valve locations are still not appropriate.
C3.0	Please note that sanitary services shall wye into the Village main
C3.0	It is necessary to provide profile sheets for Village water and sanitary mainline installations
C3.0	Create a conflict chart for all utilities that cross each other. For example if a sanitary sewer crosses under a watermain, the top of the sanitary main shall be listed and the bottom of the watermain shall be listed. If a conflict occurs a correction shall be listed (ie lower watermain, install water quality casing, etc...)
L1.0	The Village has reviewed the tree removal plan and calculated that the reforestation fee is currently \$263,450.00. Per code the removal dbh is calculated at 2814.5 inches, while the site calls out for 180 inches of replacement. A 2,634.5 dbh difference @ \$100 per dbh = \$263,450.00.
L1.0	List plant sizes for trees (caliper), shrubs (gal) and perennial (gal)

- L1.0 List seed quantities (acre)
- L1.0 List plug quantities (each)
- Misc. Should the developer wish to not utilize the Village wetland complex for storage, it will be necessary for the developer to construct the detention facility to Village standards, and deed that outlot to the Village after the 3 years of required maintenance and monitoring. This is necessary as the facility must be maintained to the highest level of care as it is directly contributory to the Village Randall Road Wetland Complex.

Should Detention Remain Onsite the following comments shall be addressed

1. Maintain a maximum 3:1 slope and provide a safety ledge at normal water level
2. List both the NWL and HWL elevations
3. Show restrictor structure (6 foot dia w/ restrictor plate & orifice opening protected with a trash guard)
4. Indicate emergency overland flood route and indicate exit slope protection such as rip rap, geogrid, plantable paver etc.. (Village prefers a plantable solution)
5. The swale along the south property line shall have a protected slope as it enters the detention facility. A better solution would be to gather flow in a structure just prior to facility and pipe to a FES within the facility. That would greatly reduce scour.
6. Provide seed mix designs for slopes (3 zone minimum)
7. Provide wetland plug types

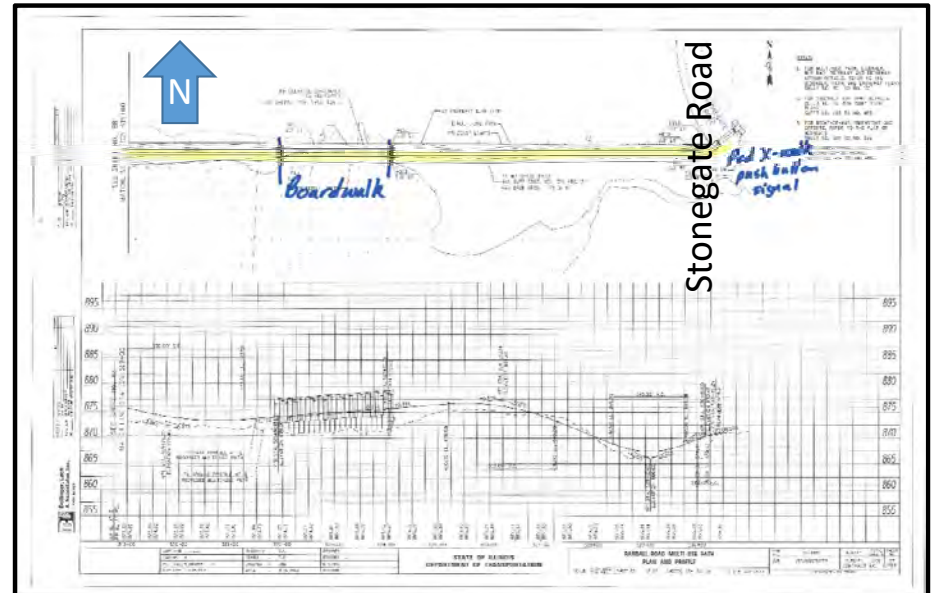
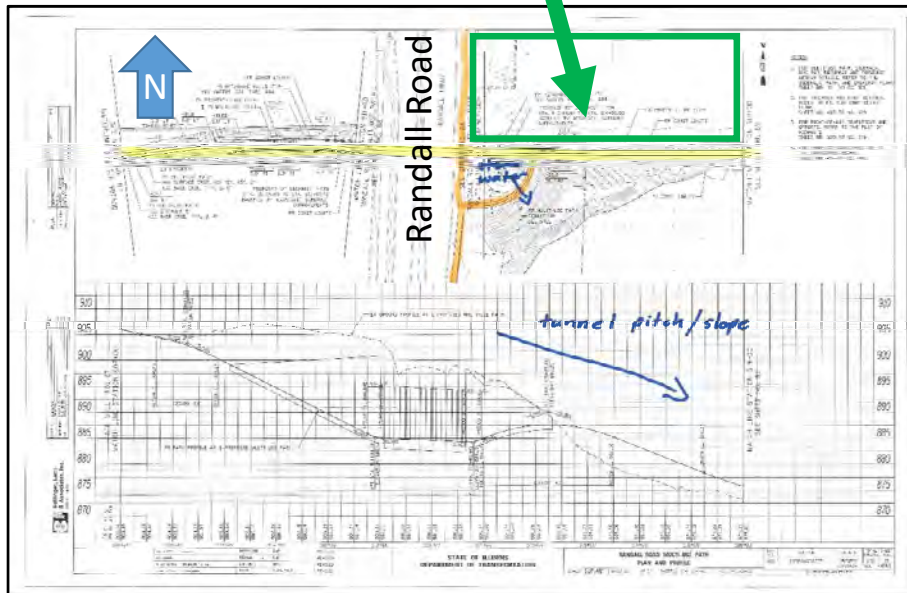
Cc: Project File (listed in footer)

Attachments: Valve Exhibit

Randall Road Pedestrian Underpass Plans

Underpass / Path in yellow;
Surplus Parcel (Lot 2)

Underpass / Path continuing east
toward Stonegate Road



May 7, 2018

Village of Algonquin
2200 Harnish Drive
Algonquin, IL 60102

Subject: CarMax Auto Superstore – Algonquin, IL; Parcel # 19-32-100-005-0000

The purpose of this letter is to outline the development and operation of a proposed CarMax Auto Superstore to be constructed on the existing Duro-Life property. CarMax is requesting a rezoning to B-2, Special Use Permit for Used Auto Sales, Preliminary/Final Plat for three lots, Preliminary/Final PUD for the CarMax and detention lot and Preliminary PUD for the remaining undeveloped lot. The CarMax site will contain a pre-owned motor vehicle sales building and attached service building.

Proposed Development:

The proposed development consists of the construction of a CarMax pre-owned automobile dealership, service building with wet bay, associated access drives, parking lot, paved areas and landscaped areas. The site currently contains a warehouse/office building with parking and access drives, which will be demolished prior to construction.

Site Layout

The proposed site design includes two customer and employee access drives. Both accesses are located along Huntington Drive with the closest access to Randall Road designed as a right-in only, the existing access serving the Duro-Life development will remain as a full access. The sales inventory display area is proposed to be located along Randall Road and will be secured by guardrail with the Corten finish and embassy-style gates for security purposes. Customer and employee parking will be located along Huntington Drive. The sales, presentation and service building will be located east of the sales display area with customer access from the parking lot on the north side of the building. The attached service building will be located south of the sales building and east of the sales display area. The sales staging area will be located east of the service building and will be surrounded by a six foot high masonry wall for screening and security purposes. Access to the sales staging area will also be secured with embassy-style security gates.

Architecture

The proposed building façade is constructed of earth-tone colored brick with an integrally colored split face CMU block base and large glazing areas. Massing is articulated by a tonal color banding in the brick veneer along with a material change created by the transition from CMU to Brick. The architectural treatments are applied consistently to all building facades to create a cohesive look. Variated roof forms have been incorporated to distinguish the main customer entry points to the building. These entry features are constructed of white EIFS pilasters and a blue standing seam gable roof with the CarMax logo above the entry doors. Roof-top equipment is screened by a pre-finished earth-tone metal RTU screen and parapet walls.

Landscape

Landscaping has been incorporated into the customer and employee parking lot and around the perimeter of the site. Landscaping includes deciduous trees and shrubs, evergreen shrubs, sod and rock mulch. The Corten guardrail will be screened by evergreen shrubs. Any existing trees within the property that may be saved will remain.

The following information outlines the proposed operations based on similarly operating CarMax facilities.

Hours of Operation

Store management will set operating hours closer to the opening date; however, the showroom (sales) areas are typically open to the public Monday through Saturday from 9:00 a.m. to 9:00 p.m. with limited hours on Sundays, if permitted by local law. The retail service areas are typically open to the public Monday through Friday from 7:30 a.m. to 6:00 p.m. Associates will be present at the store several hours before and after the public operating hours. This store is expected to employ up to 25 associates.

Deliveries

Deliveries of vehicles, parts and supplies are made on-site and typically require the presence of associates to receive the delivery. Vehicle carriers will enter the site and load and unload vehicles in the designated area on the south side of the customer and employee parking lot. Unloaded vehicles will be driven by employees from the parking lot into the staging area to await preparation for resale. An estimated 4 to 6 tractor trailers per day are anticipated as part of the operation.

Sales & Marketing

CarMax operates differently from traditional car dealerships in that it physically separates its inventory area from customer and employee parking. This is both for asset protection as well as operational efficiency and public safety. All inventory display areas are separated from the general public by means of guardrails, gates and fencing. Ornamental wrought-iron style fencing is used to separate the customer and employee parking from the sales display area.

Vehicular access to the sales display area is controlled by embassy-style security gates through the use of a secured key-card. Only employees are permitted to drive cars within the sales display area. Emergency access will be provided within staging and sales display area as required by the local fire department.

CarMax's business model is to promote a high-end retail experience, and a welcoming environment to their customers and associates. As such CarMax does not use outdoor loudspeakers as associates carry pagers and / or cell phones for communications. In addition, CarMax does not use flags, balloons, inflatable gorillas (or any other animal), placards in open car hoods, painted window lettering or the like in its marketing.

Service Operations

CarMax currently offers limited retail vehicle service (routine maintenance, tires, diagnostic and mileage services) and provides repairs of vehicles covered by their extended service plans. All service work is performed inside fully-conditioned buildings equipped with rollup doors, providing the associates with a great work environment and eliminating the need to conduct operations with open bay doors.

Retail service vehicles and vehicles awaiting disposition off-site are stored in the secured non-public staging area on a temporary basis. As a visual screen and to provide security for these vehicles, the staging area is surrounded by a six-foot-high masonry wall. Vehicular access to that area is strictly controlled

through the use of embassy-style security gates. Since the staging and storage of vehicles within this area is constantly changing on a daily basis, parking spaces are not designated on the plan.

An above ground fuel storage tank with a non-public fuel pump is proposed for this site. The tank and fuel pump will be located inside the secured non-public sales staging area to fuel inventory vehicles as needed.

Site Lighting & Security

CarMax uses LED lighting fixtures mounted on 26-foot tall light poles for visibility and security. Fixtures are full cut off and downcast to reduce light spill onto adjacent properties. Exterior lighting will be reduced after operating hours.

CarMax typically does not use on-site security guards, but uses interior and exterior security cameras for safety and inventory protection.

In summary, CarMax looks forward to partnering with the City of Algonquin and its residents in constructing and operating a successful new store.

If you have any questions, or need additional information, please feel free to give me a call at (720) 445-4382.

Thank you,



Greg Saia
Development Manager

CC: JM Dixon, CarMax Auto Superstores, Inc.

ATTACHMENT A

1. As to proposed Lot 1 of the Subdivision, Applicant requests the following:

A Special Use that allows said Lot 1 to be used in connection with motor sales (both new and/or used) and for all activities accessory to said principal use, including, without limitation, (i) motor vehicle service and repair, (ii) vehicle financing and leasing, (iii) vehicle appraisals and acquisitions, (iv) outdoor vehicle inventory storage, (v) outdoor motor fuel storage and dispensing (not open to the public), and (vi) vehicle inventory cleaning (through a wet-bay) and detailing. At the election of the operator of the vehicle sales facility, a non-public car wash would be permitted to (i) clean vehicle inventory for sale and (ii) clean the vehicles of customers.

2. As to Lot 2:

- (A) The Permitted Uses would be:

- (i) All Permitted Uses as currently allowed under the Village's B-2 Business District, General Retail (which permitted uses shall include those allowed in past Permitted Uses under the Village's B-1 Business District, Limited Retail).

- (ii) The following additional "Permitted Uses":

1. Hotels and Motels
 2. Libraries
 3. Motion Picture Theatres
 4. Pharmacy with or without a drive through facility
 5. Schools (public or private)

- (B) The Special Uses would be:

- (i) All Special Uses as currently allowed under the Village's B-2 Business District, General Retail.

- (ii) The following additional "Special Uses":

1. Automotive Repair
 2. Auto Parts and Tire Sales
 3. Child Daycare Services
 4. Carryout Restaurants with drive through facility
 5. Packaged Liquor Store
 6. Car Wash open to the general public
 7. Gasoline station with or without mini-mart sales

FINAL PLAT OF SUBDIVISION CARMAX 6125 SUBDIVISION

PART OF THE NORTHWEST QUARTER OF SECTION 32, TOWNSHIP 43 NORTH,
RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN

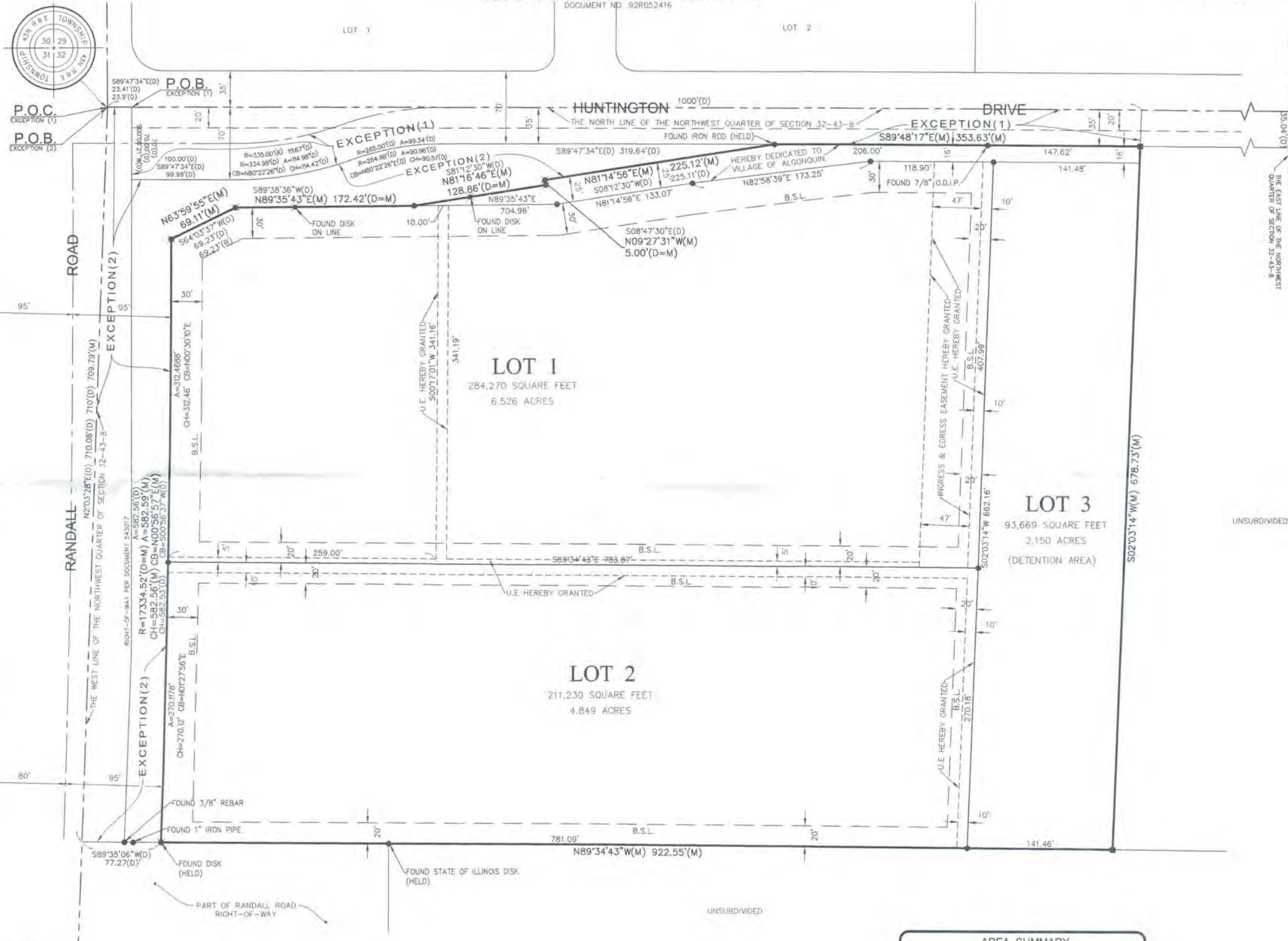
KAPER'S 1ST RESUBDIVISION OF LOT 10 IN KAPER'S EAST SUBDIVISION
DOCUMENT NO. 92R052416

GRAPHIC SCALE



(IN FEET)
1 inch = 50 ft

P.I.N. 19-32-100-005



RIGHT-OF-WAY PER DOCUMENT 54307
DOCUMENT NO. 2007010984

ABBREVIATIONS

O.D.I.P. = OUTSIDE DIAMETER IRON PIPE
(R) = RECORD BEARING OR DISTANCE
(M) = MEASURED BEARING OR DISTANCE
(C) = CALCULATED BEARING OR DISTANCE
(D) = DEED BEARING OR DISTANCE
B.S.L. = BUILDING SETBACK LINE
U.E. = UTILITY EASEMENT
D.E. = DRAINAGE EASEMENT
P.U.E. = PUBLIC UTILITY EASEMENT
P.O.C. = POINT OF COMMENCEMENT
P.O.B. = POINT OF BEGINNING
P.U. & D.E. = PUBLIC UTILITY AND DRAINAGE EASEMENT

LEGEND

- SET 7/8" O.D.I.P. SET UNLESS OTHERWISE NOTED
- FOUND DISK LABELED "180H-2797" UNLESS OTHERWISE NOTED
- SET CONCRETE MONUMENT UNLESS OTHERWISE NOTED
- SET CROSS IN CONCRETE UNLESS OTHERWISE NOTED

LINE LEGEND

- SUBDIVISION BOUNDARY LINE
- ADJACENT LAND PARCEL LINE
- LOT LINE
- EASEMENT LINE
- CENTERLINE
- BUILDING SETBACK LINE
- SECTION LINE

AREA SUMMARY	
GROSS	602,251 SQUARE FEET OR 13.826 ACRES
R.O.W. DEDICATION	13,081 SQUARE FEET OR 0.300 ACRES
NET AREA	589,170 SQUARE FEET OR 13.526 ACRES (TO HEAVY LINES) (BASED ON MEASURED VALUES)

NO.	DATE	BY	PG.
1	5-1-18	MRA	
REVISIONS			
REVISED PER LETTER DATED 4-30-18			
DATE	4-12-18	PC	DR
CHECKED BY	SK	DATE	5-1-18
BOOK		DATE	5-1-18

CLIENT
Kimley»Horn
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1001 WARRENDALE ROAD, SUITE 350
ALTON, ILLINOIS 61810-5550
WWW.KIMLEY-HORN.COM

PROJECT
**CARMAX 6125
SUBDIVISION**
2401 N. Huntington Drive
Algonquin, Illinois

COMPASS
SURVEYING LTD.
ALTA SURVEYS • TOPOGRAPHY • CONSTRUCTION STAKING
2631 GINGER WOODS PARKWAY, STE. 100
ALTON, ILLINOIS 61810-5550
PHONE: (618) 230-6100 FAX: (618) 230-7030 EMAIL: ADMIN@COMPASSSURVEYING.COM

SCALE: 1" = 50'
1 OF 2
PROJ. NO.: 18.0113

Drawing name: K:\CHS_LUCA\16860001_Centerpoint_Approval_L1.0_Design_CAD\NewSheet\LANDSCAPE PLAN.dwg, rendered: 4/16/2018 14:43pm, by: Amanda Ellis
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ORIGINAL ISSUE:
5/7/18
KHA PROJECT NO.
168608001
SHEET NUMBER
L1.0

CARMAX
2407 N. HUNTINGTON DR.
ALGONQUIN, IL 60102

ILLUSTRATIVE
PLAN

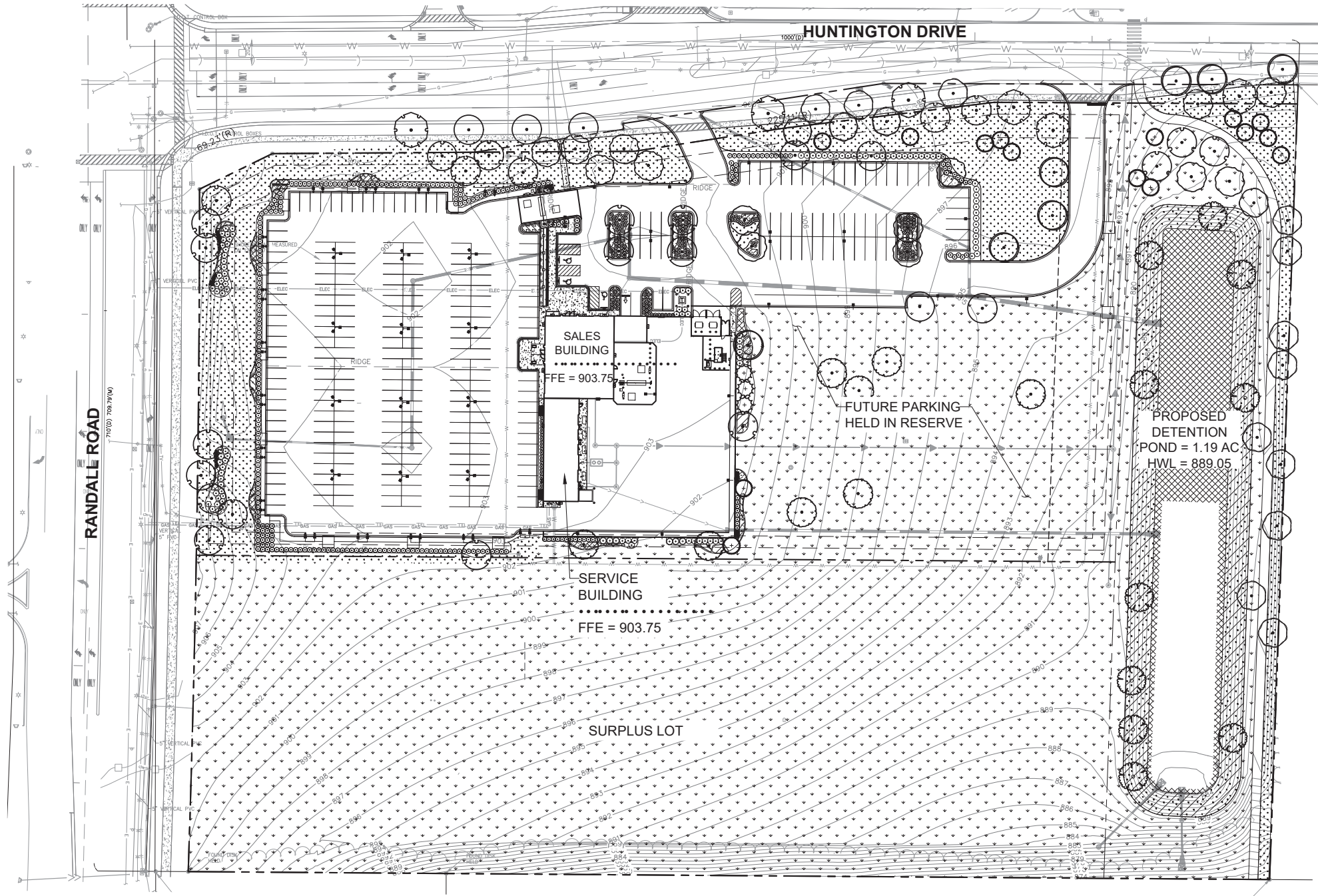


SCALE: AS NOTED
DESIGNED BY: MES
DRAWN BY: KRN
CHECKED BY: MES

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1001 WARRENVILLE ROAD, SUITE 350,
Lisle, IL 60532-8077-5550
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No.	REVISIONS	DATE	BY
1	6/21/18	KHA	

Drawing name: K:\CHS_DEV\168608001_centerpoint_algonquin_3\2 Design\CAD\plansheets\LANDSCAPE PLAN.dwg L2.0 LANDSCAPE PLAN Jul 16, 2018 1:41pm by AmandaFello
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LANDSCAPE DATA

ROW LANDSCAPING
TREES SPACED 40 FT ON CENTER ALONG ROW. PLANTED 35 FT FROM STREET CORNERS.
HUNTINGTON DR. = 610/40= 15 TREES REQUIRED; 15 PROVIDED ALONG HUNTINGTON DR.

PERIMETER LANDSCAPE
TREES SPACED 40 FT ON CENTER ALONG THE ABUTTING PROPERTY LINE (DOES NOT INCLUDE ROW AREA)
RANDALL ROAD= 300 LF/40= 8 (7.5) TREES REQUIRED; 8 PROVIDED ALONG RANDALL
HUNTINGTON DR. = 610/40= 15 TREES REQUIRED; 15 PROVIDED ALONG HUNTINGTON DR.

PARKING LOTS LOCATED AT FRONT OF PRIMARY BUILDING SHALL BE SCREENED WITH HEDGE
.....
SCREEN HEDGE PROVIDED

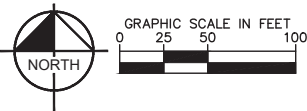
BUILDING FOUNDATION PLANTING
REQUIRED FOR ALL BUILDINGS
PROVIDED WHERE SPACE AVAILABLE (ACROSS FROM ADA STALLS)

NON-RESIDENTIAL LANDSCAPING MIN STANDARDS
LANDSCAPE SHALL BE 20% TO TOTAL LOT AREA NUMBERS NOT TO INCLUDE DETENTION OR RETENTION AREAS
TOTAL DEVELOPED LOT AREA= 233,917 SF
20% OF 233,917= 46,783 SF REQUIRED
AREA PROVIDED = 88,514 SF

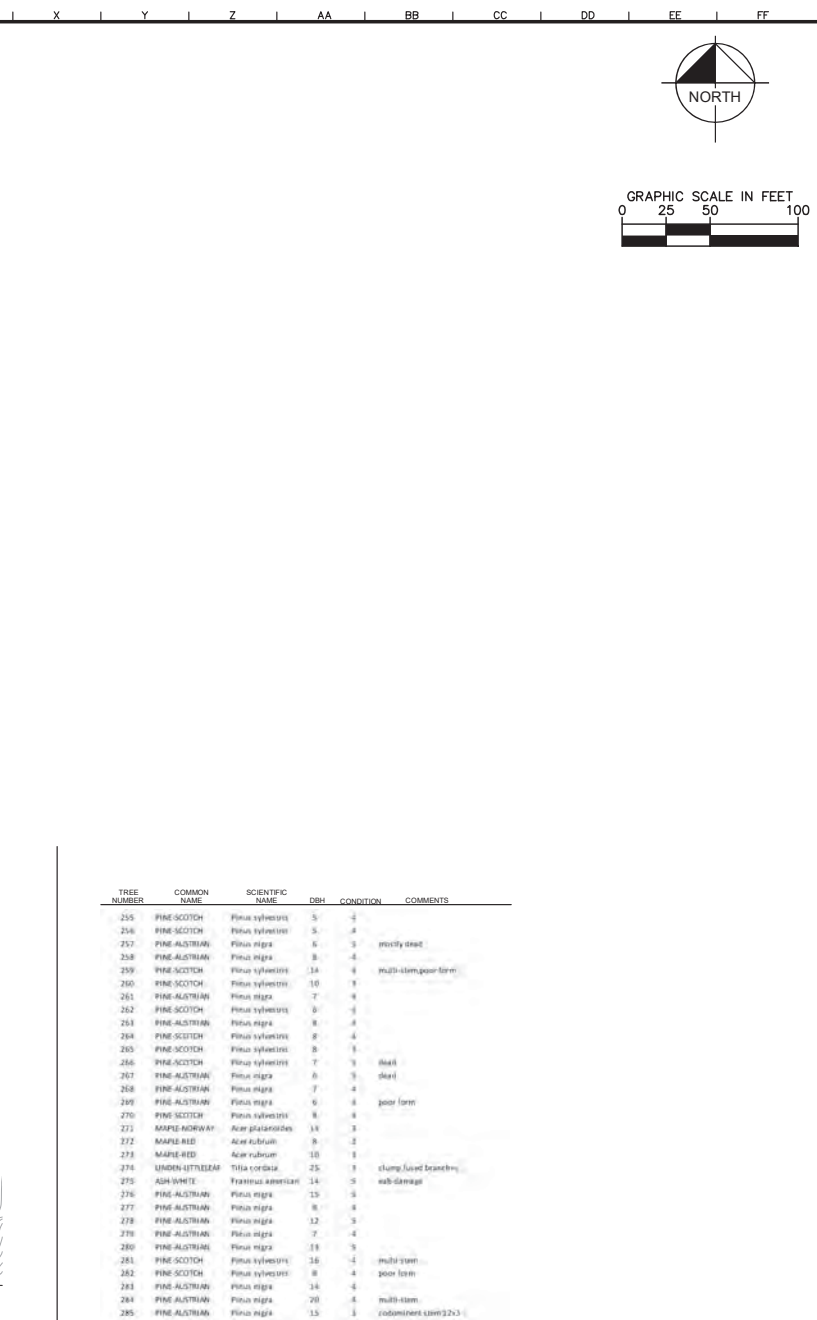
STORMWATER MANAGEMENT AREAS

WETLAND PLANTING EDGE PROVIDED IN LIEU OF HARD EDGE TREATMENT
OTHER LANDSCAPE IMPROVEMENTS PROVIDED AROUND PERIMETER

EXISTING LANDSCAPING
TREE SURVEY SHALL BE COMPLETED AND TREES TO REMAIN WILL BE ACCOUNTED FOR IN THE LANDSCAPE PLANT QUANTITIES

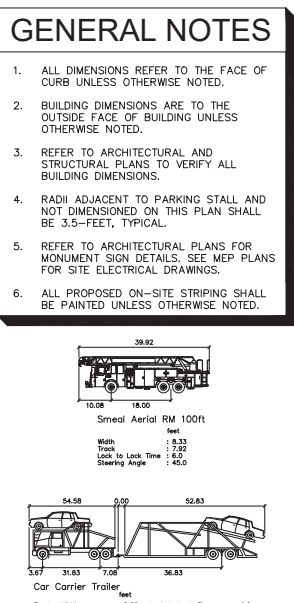
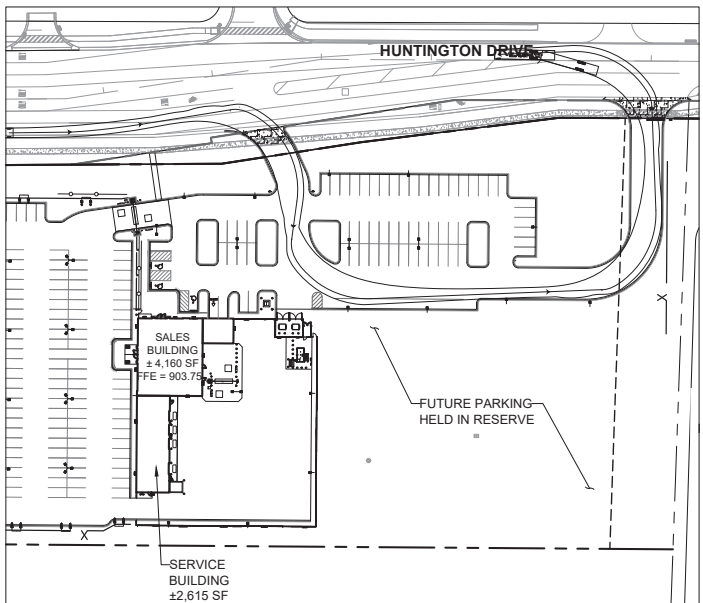
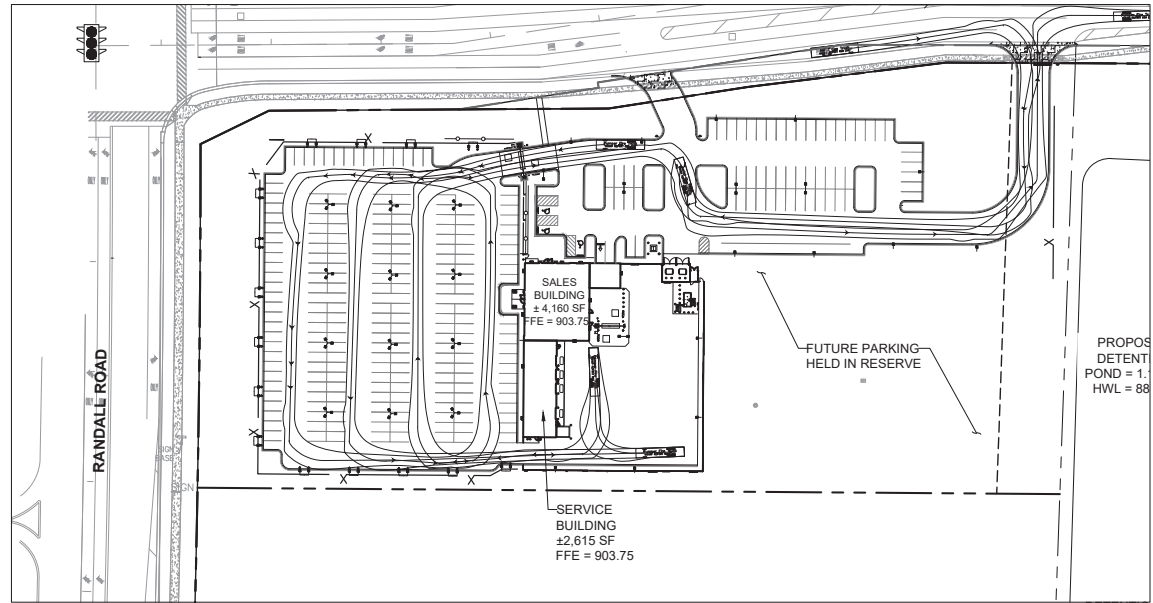
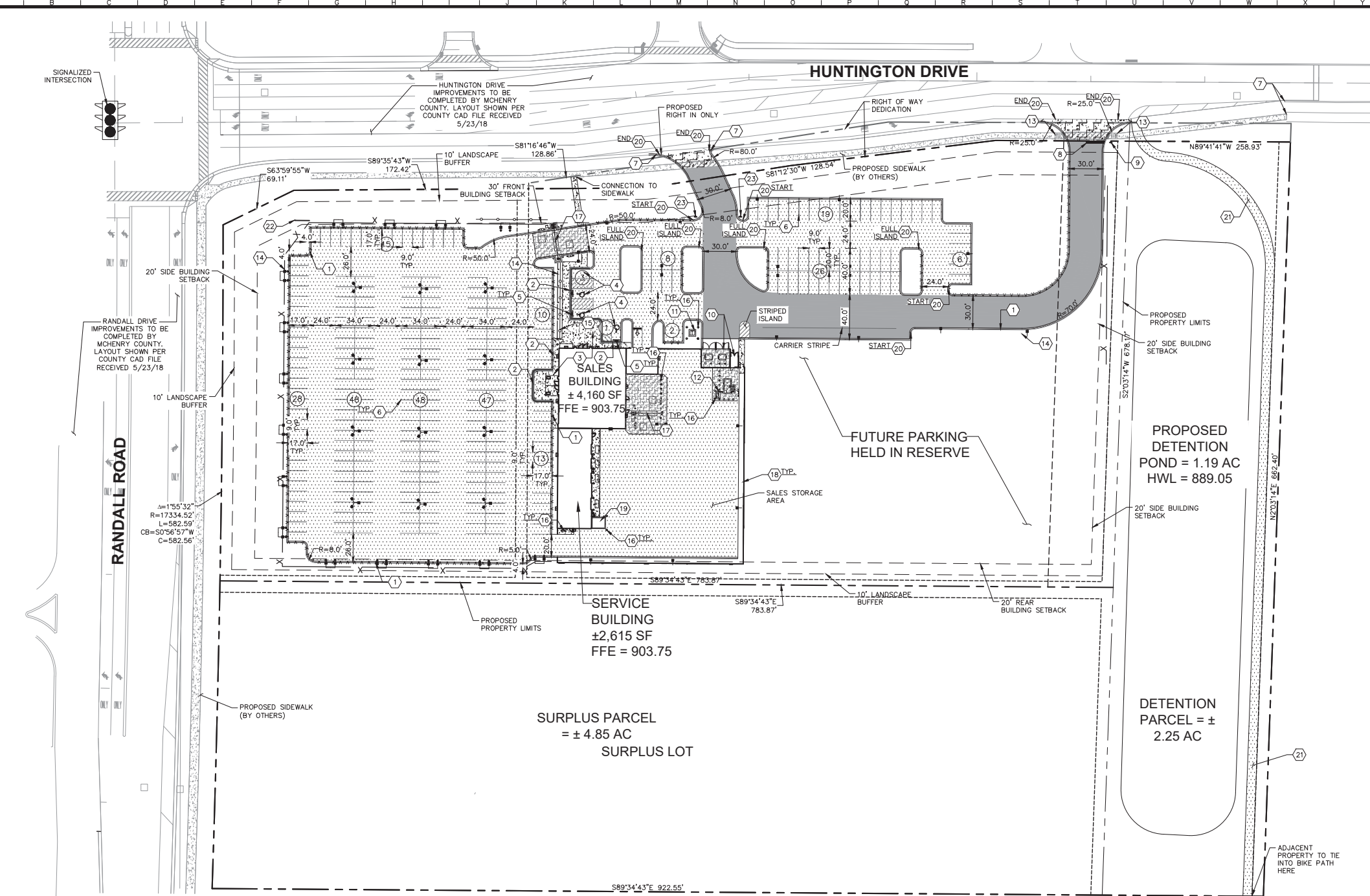


CARMAX		LANDSCAPE PLAN		Kimley»Horn
2401 N. HUNTINGTON DR. ALGONQUIN, IL 60102				
ORIGINAL ISSUE: 5/7/18		AS NOTED		
KHA PROJECT NO. 168608001		DESIGNED BY: MES		
SHEET NUMBER		DRAWN BY: KRN		
L1.0		CHECKED BY: MES		
		* 2018 KIMLEY-HORN AND ASSOCIATES, INC. 1001 WARRENVILLE ROAD, SUITE 350, BLOOMINGDALE, IL 60010 PHONE: 630-487-5550 WWW.KIMLEY-HORN.COM		
		AGENCY COMMENTS		
		REVISIONS		
		DATE		
		BY		
		KHA		
		No.		



<p>CARMAX</p> <p>2401 N. HUNTINGTON DR. ALGONQUIN, IL 60102</p>	<p>EXISTING TREE PLAN</p>		<p>SCALE: AS NOTED</p> <p>DESIGNED BY: MES</p> <p>DRAWN BY: KRN</p> <p>CHECKED BY: MES</p>	<p>Kimley»Horn</p> <p>© 2018 KIMLEY-HORN AND ASSOCIATES, INC. 1001 WARRENVILLE ROAD, SUITE 350, BIRMINGHAM, AL 35243 PHONE: 630-487-5550 WWW.KIMLEY-HORN.COM</p>	No.	REVISIONS	DATE	BY

Drawing name: K:\GIS\DEV\168608001_centerpoint_algonquin_3\2 Design\CAD\plansheets\C1.0 - SITE PLAN.dwg Jun 22, 2018 9:14am by AmandaFalla
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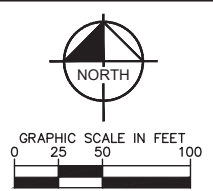


SITE DATA	
PROJECT DESCRIPTION:	NEW CONSTRUCTION OF CARMAX SUPERSTORE
LOCATION:	SECTION 32, TOWNSHIP 43 N, RANGE 8 E
ADDRESS:	2401 N. HUNTINGTON DR. ALGONQUIN, IL 60102
ZONING DISTRICT:	PD - PLANNED DEVELOPMENT
LAND USE:	COMMERCIAL
FLOOD ZONE:	NOT APPLICABLE
SITE AREA:	13.53
NET DEVELOPED AREA:	6.53 AC
MAX BUILDING HEIGHT:	35' - 3 STORIES
LOT COVERAGE:	
BUILDING AREA:	0.17 AC (7,405 SF)
IMPERVIOUS AREA:	3.23 AC (140,699 SF)
PERVIOUS AREA/LANDSCAPED AREA:	3.13 AC (135,907 SF)
BUILDING SETBACKS:	
FRONT	30'
REAR	20'
SIDE	20'
LANDSCAPE BUFFERS:	
FRONT	10'
REAR	10'
SIDE	10'

PARKING SUMMARY	
PARKING SPACES PROVIDED:	
SALES	= 209 SPACES
C/E	= 64 SPACES
W/P	= 58 SPACES
TOTAL:	= 331 SPACES
ADA STALLS REQUIRED:	= 3 STALLS
ADA STALLS PROVIDED:	= 3 STALLS

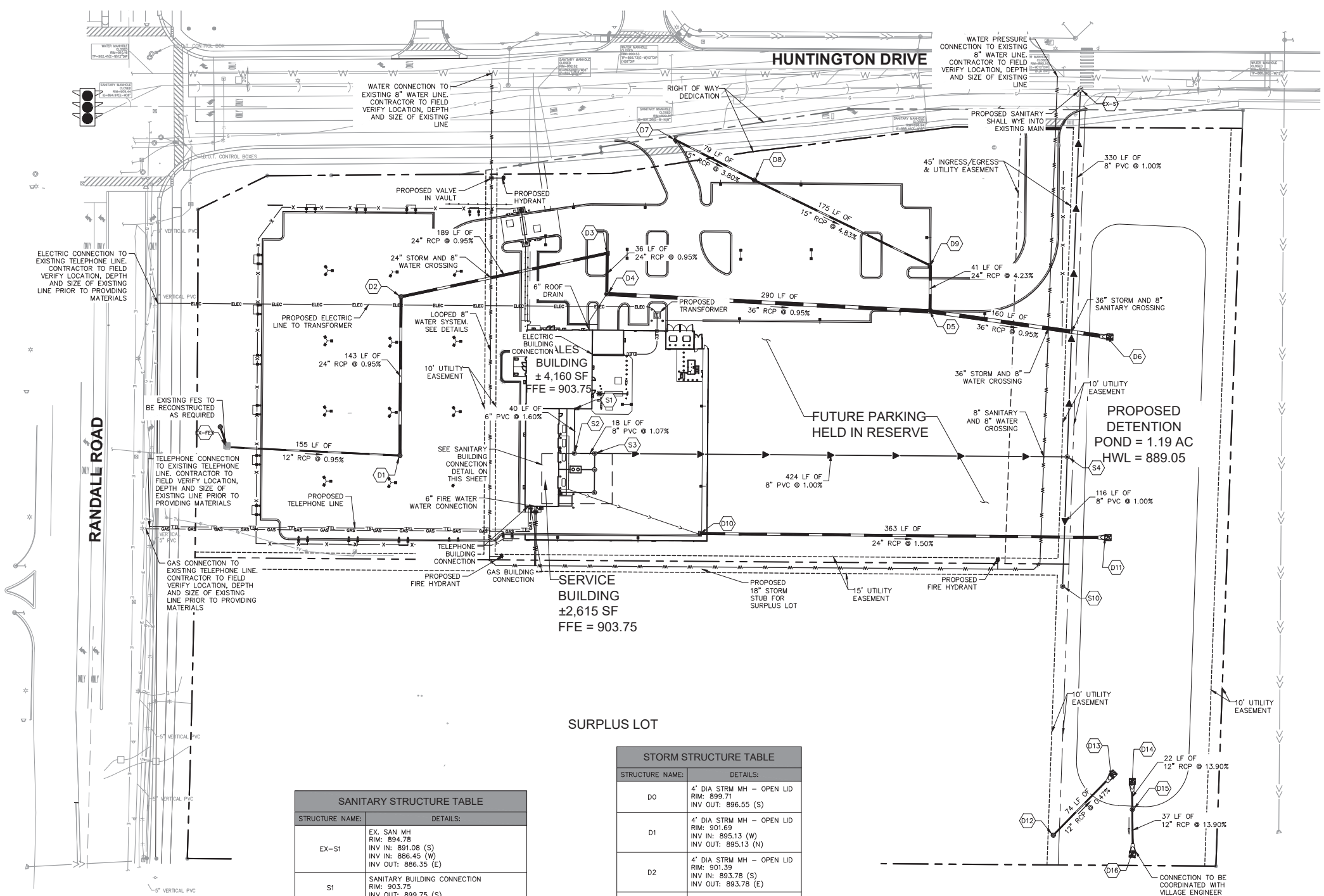
LEGEND	
	STANDARD DUTY ASPHALT PAVEMENT SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
	HEAVY DUTY ASPHALT PAVEMENT SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
	CONCRETE SIDEWALK SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
	HEAVY DUTY CONCRETE PAVEMENT SEE CONSTRUCTION DETAILS FOR PAVEMENT SECTION
	STANDARD PITCH CONCRETE CURB AND GUTTER
	FLUSH CURB AND GUTTER
	REVERSE PITCH CURB AND GUTTER
	GUARD RAIL
	ORNAMENTAL FENCE

KEY NOTES	
1	B6.12 CONCRETE CURB AND GUTTER, TYP.
2	DEPRESSED CURB AND GUTTER
3	CONCRETE SIDEWALK, TYP.
4	ACCESSIBLE PAVEMENT MARKINGS, TYP.
5	ACCESSIBLE PARKING SIGN, TYP. (MUTCD R7-8)
6	4" WIDE PAINTED SOLID LINE, TYP.
7	CONNECT TO EXISTING PAVEMENT, SIDEWALK, CURB, TYP.
8	24" WIDE STOP BAR, TYP.
9	STOP SIGN, TYP. (MUTCD R1-1)
10	DUMPSTER AND TIRE ENCLOSURE
11	TRANSFORMER PAD
12	ABOVE GROUND FUEL TANK
13	ACCESSIBLE RAMP
14	GUARD RAIL WITH CORTEN FINISH
15	ORNAMENTAL FENCE
16	BOLLARD, TYP.
17	POP-UP SECURITY GATE WITH KNOX BOX
18	6" HIGH CMU WALL, TYP.
19	ABOVE GROUND FUEL TANK
20	MOUNTABLE CURB AND GUTTER
21	BIKE PATH
22	MONUMENT SIGN
23	DO NOT ENTER SIGN



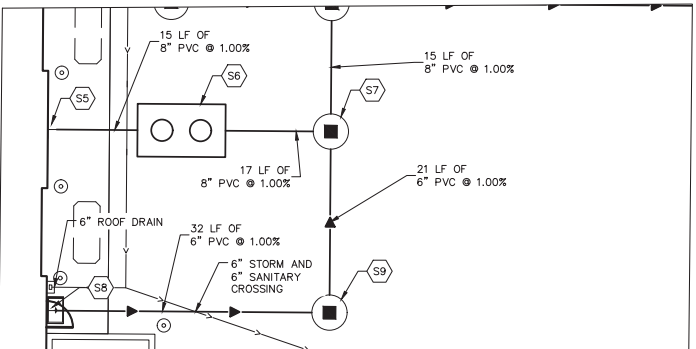
KIMLEY-HORN		6/21/18	KHA	BY
© 2018 KIMLEY-HORN AND ASSOCIATES, INC. 1500 N. HUNTINGTON ROAD, SUITE 350, ALGONQUIN, IL 60102. PHONE: 630-487-5550 WWW.KIMLEY-HORN.COM		AGENCY COMMENTS		
SCALE: AS NOTED		REVISIONS		
DESIGNED BY: MES		DATE		
DRAWN BY: KRN		SHEET NUMBER		
CHECKED BY: MES		C1.0		
ORIGINAL ISSUE: 5/7/18		KHA PROJECT NO. 168608001		
CARMAX		2401 N. HUNTINGTON DR. ALGONQUIN, IL 60102		

Drawing name: K:\GIS\DEV\168608001_centerpoint_layout\3\2 Design\CAD\plansheets\UTILITY PLAN.dwg Jun 22, 2018 9:14am by: AnnalisaFetig
This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



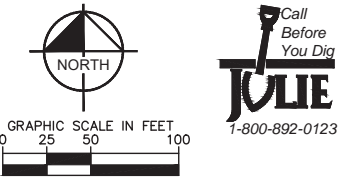
SANITARY STRUCTURE TABLE	
STRUCTURE NAME:	DETAILS:
EX-S1	EX. SAN MH RIM: 894.78 INV IN: 891.08 (S) INV IN: 886.45 (W) INV OUT: 886.35 (E)
S1	SANITARY BUILDING CONNECTION RIM: 903.75 INV OUT: 899.75 (S)
S2	4' DIA SAN MH RIM: 903.61 INV IN: 899.11 (N) INV OUT: 899.01 (E)
S3	4' DIA SAN MH RIM: 903.39 INV IN: 898.82 (S) INV IN: 898.81 (W) INV OUT: 898.71 (E)
S4	4' DIA SAN MH RIM: 895.67 INV IN: 894.48 (W) INV OUT: 894.38 (N) INV OUT: 888.17 (S)
S5	SANITARY BUILDING CONNECTION RIM: 903.75 INV OUT: 899.58 (E)
S6	GREASE TRAP RIM: 903.56 INV IN: 899.43 (W) INV OUT: 899.23 (E)
S7	MH RIM: 903.32 INV IN: 899.06 (W) INV IN: 899.12 (S) INV OUT: 898.96 (N)
S8	SANITARY BUILDING CONNECTION RIM: 903.75 INV OUT: 899.75 (E)
S9	4' DIA SAN MH RIM: 903.25 INV IN: 899.43 (W) INV IN: 899.33 (N)
S10	SAN MH FOR FUTURE SURPLUS CONNECTION RIM: 891.50 INV IN: 887.01 (N)

STORM STRUCTURE TABLE	
STRUCTURE NAME:	DETAILS:
D0	4' DIA STRM MH - OPEN LD RIM: 899.71 INV OUT: 896.55 (S)
D1	4' DIA STRM MH - OPEN LD RIM: 901.69 INV IN: 895.13 (W) INV IN: 895.13 (N)
D2	4' DIA STRM MH - OPEN LD RIM: 901.39 INV IN: 893.78 (S) INV OUT: 893.78 (E)
D3	CURB MH RIM: 901.90 INV IN: 891.98 (W) INV OUT: 891.98 (S)
D6	FES INV IN: 887.35 (W)
D7	2' DIA CURB INLET RIM: 900.96 INV OUT: 898.51 (SE)
D9	CURB MH RIM: 895.62 INV IN: 887.04 (NW) INV IN: 887.04 (S)
D10	2' DIA CURB INLET RIM: 901.41 INV OUT: 893.23 (E)
D11	FES INV IN: 887.79 (W)
D12	SWALE INLET RIM: 883.41 INV OUT: 882.24 (NE)
D13	FES INV IN: 881.90 (SW)
D14	FES INV OUT: 885.27 (S)
D15	OUTLET CONTROL STRUCTURE RIM: 888.67 INV IN: 882.17 (N) INV IN: 882.17 (S)
D16	FES INV IN: 877.00 (N)
EX-FES	EXISTING FLARED END SECTION INV IN: 896.45 (N) INV OUT: 896.60 (E)



UTILITY LEGEND	
	EX. WATER LINE
	EX. HYDRANT
	EX. WATER VALVE
	EX. SANITARY SEWER LINE
	EX. SANITARY SEWER MANHOLE
	EX. SANITARY SEWER CLEANOUT
	EX. STORM DRAIN LINE
	EX. STORM MANHOLE
	EX. STORM STRUCTURE/INLET
	EX. GAS LINE
	EX. GAS METER
	EX. UNDERGROUND ELECTRIC LINE
	EX. UNDERGROUND TELEPHONE LINE
	EX. LIGHT POLE
	PROPOSED UNDERGROUND ELECTRIC LINE
	GAS LINE (BY GAS COMPANY)
	PROPOSED PHONE LINE
	PROPOSED STORM SEWER LINE
	PROPOSED OPEN LID STORM STRUCTURE (PAVEMENT USE NEENAH R-2540) (GRASS USE NEENAH R-4340-B BEEHIVE)
	PROPOSED CLOSED LID STORM STRUCTURE (PAVEMENT USE NEENAH R-1772) (GRASS USE NEENAH R-1786)
	PROPOSED OPEN LID CURB STRUCTURE (B6.12 C&G USE NEENAH R-3281-A)
	PROPOSED SANITARY SEWER LINE
	PROPOSED SANITARY MANHOLE
	PROPOSED STORM/SANITARY CLEANOUT
	PROPOSED WATER LINE
	PROPOSED VALVE VAULT
	PROPOSED VALVE BOX
	PROPOSED FIRE HYDRANT
	PROPOSED LIGHT POLE
	PROPOSED TRANSFORMER PAD (FOR REFERENCE ONLY)
	RIP RAP

UTILITY NOTES	
GENERAL UTILITY NOTES	
1.	ALL WATER LINES \geq 3" SHALL BE DUCTILE IRON PIPE, CLASS 52.
2.	ALL SANITARY SEWER LINES SHALL BE PVC MEETING, ASTM D-3034 SDR 26 EXCEPT FOR SANITARY SEWER THAT CROSSES ABOVE WATER MAIN, THIS PIPE SHALL BE AWWA C900 (UNLESS WATER MAIN CASING IS UTILIZED). PROVIDE 42" MINIMUM COVER.
3.	CONTRACTOR SHALL COORDINATE ANY DISRUPTIONS TO EXISTING UTILITY SERVICES WITH ADJACENT PROPERTY OWNERS.
4.	ALL ELECTRIC AND TELEPHONE EXTENSIONS INCLUDING SERVICE LINES SHALL BE CONSTRUCTED TO THE APPROPRIATE UTILITY COMPANY SPECIFICATIONS. ALL UTILITY DISCONNECTIONS SHALL BE COORDINATED WITH THE DESIGNATED UTILITY COMPANIES.
5.	CONSTRUCTION SHALL NOT START ON ANY PUBLIC UTILITY SYSTEM UNTIL WRITTEN APPROVAL HAS BEEN RECEIVED BY THE ENGINEER FROM THE APPROPRIATE GOVERNING AUTHORITY AND CONTRACTOR HAS BEEN NOTIFIED BY THE ENGINEER.
6.	CONTRACTOR TO CALL "JULIE" (1-800-892-0123) TO COORDINATE FIELD LOCATIONS OF EXISTING UNDERGROUND UTILITIES BEFORE ORDERING MATERIALS OR COMMENCING CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES IMMEDIATELY.
7.	PRIOR TO THE CONSTRUCTION OF OR CONNECTION TO ANY STORM DRAIN, SANITARY SEWER, WATER MAIN OR ANY OTHER UTILITIES, THE CONTRACTOR SHALL EXCAVATE, VERIFY AND CALCULATE ALL POINTS OF CONNECTION AND ALL UTILITY CROSSINGS AND INFORM THE ENGINEER AND THE OWNER. DEVELOPER OF ANY CONFLICT OR REQUIRED DEVIATIONS FROM THE PLAN. NOTIFICATION SHALL BE MADE A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION. THE ENGINEER AND ITS CLIENTS SHALL BE HELD HARMLESS IN THE EVENT THAT THE CONTRACTOR FAILS TO MAKE SUCH NOTIFICATION. THE MUNICIPALITY SHALL BE NOTIFIED OF ANY AND ALL CHANGES TO THE DESIGN PLANS.
8.	CONTRACTOR SHALL COMPLY COMPLETELY WITH THE LATEST STANDARDS OF OSHA DIRECTIVES OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES. THE CONTRACTOR SHALL USE SUPPORT SYSTEMS, SLOPING, BENCHING AND OTHER MEANS OF PROTECTION. THIS IS TO INCLUDE, BUT NOT LIMITED TO ACCESS AND EGRESS FROM ALL EXCAVATION AND TRENCHING. CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH PERFORMANCE CRITERIA AS REQUIRED BY OSHA.
9.	CONTRACTOR TO AVOID DISRUPTION OF ANY ADJACENT TENANT'S TRAFFIC OPERATIONS DURING INSTALLATION OF UTILITIES.
10.	ALL DIMENSIONS ARE TO CENTERLINE OF PIPE OR CENTER OF MANHOLE UNLESS NOTED OTHERWISE.
11.	SEE ARCHITECTURAL AND MEP PLANS FOR EXACT UTILITY CONNECTION LOCATIONS AT BUILDING.
12.	LIGHT POLES SHOWN FOR COORDINATION PURPOSES ONLY AND DO NOT REPRESENT ACTUAL SIZE. SEE SITE LIGHTING PLANS BY OTHERS FOR MORE INFORMATION.
13.	STORMWATER FACILITIES MUST BE FUNCTIONAL BEFORE BUILDING CONSTRUCTION BEGINS.
14.	PROPOSED DETENTION POND HAS BEEN SITED FOR CONDITIONS AS SHOWN ON THIS SHEET. WHEN SURPLUS LOT IS DEVELOPED, DEVELOPER WILL BE REQUIRED TO TIE INTO OFFSITE REGIONAL DETENTION POND FOR ADDITIONAL STORAGE.



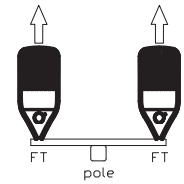
KHA	6/21/18	DATE	BY
AGENCY COMMENTS			
REVISIONS			
No.			
Kimley»Horn			
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SCALE:	AS NOTED	DESIGNED BY: MES	DRAWN BY: KRN
CHECKED BY: MES			
UTILITY PLAN			
CARMAX			
2401 N. HUNTINGTON DR. ALGONQUIN, IL 60102			
ORIGINAL ISSUE: 5/7/18			
KHA PROJECT NO. 168608001			
SHEET NUMBER			
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S RANDALL RD

ADT 42K

N HUNTINGTON DR

AREA TO BE ILLUMINATED

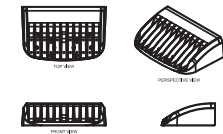


Note: Bolt-on parallel arms are designed for direct mounting of fixture to face of bracket. Standard arm is not required.

XALM
LED Area Light



XWM
LED Wall Mount



ALL 24' POLES TO BE MOUNTED ON CONCRETE PEDESTALS
2' ABOVE FINISHED GRADE FOR A TOTAL MOUNTING HEIGHT OF 26'

OPERATIONAL HOURS

Calculation Summary

Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
CALCS 2' 4' ABOVE GRADE	Illuminance	Fc	1.67	16.8	0.0	N.A.	N.A.
PROPERTY LINE ABOVE OFC @ GRAD	Illuminance	Fc	0.66	1.3	0.0	N.A.	N.A.
PROPERTY LINE BELOW OFC @ GRAD	Illuminance	Fc	0.04	0.6	0.0	N.A.	N.A.
C E LOT	Illuminance	Fc	4.35	9.6	0.1	43.50	96.00
C E LOT WITHOUT DRIVE	Illuminance	Fc	4.45	9.6	0.6	7.42	16.00
SALES LOT	Illuminance	Fc	9.07	16.8	3.5	2.59	4.80
SALES STAGING	Illuminance	Fc	5.54	9.1	1.4	3.96	6.50

LPD Area Summary

Label	Total Watts
CE LOT	2700.901
SALES	7202.399
SALES STAGING	1733.2
Total Project Watts	11636.49

Luminaire Schedule

Symbol	Qty	Label	Arrangement	Description	LLF	Lumens/Lamp	Arr. Lum. Lumens	Arr. Watts
	4	DLA1	TWIN	XALM-2-LED-SS-50-IL-20-TWINKA-40-SINGLE PREP	0.900	NA.	26070	312.4
	14	DLG	D180°	XALM-SW-LED-SS-20	0.900	NA.	38100	312
	17	DLG	SINGLE	XALM-FT-LED-HD-50-IL-20	0.900	NA.	22020	241.3
	8	DLG1	SINGLE	XALM-FT-LED-SS-50-IL-20	0.900	NA.	14362	154.1
	9	DLW2	SINGLE	XWM-FT-LED-08-50-19' MH	0.900	NA.	8654	76

Total Project Watts
Total Watts = 11636.49



LIGHTING PROPOSAL LO-141548-7

CARMAX
ALBUQUERQUE, NM
(SP-16)

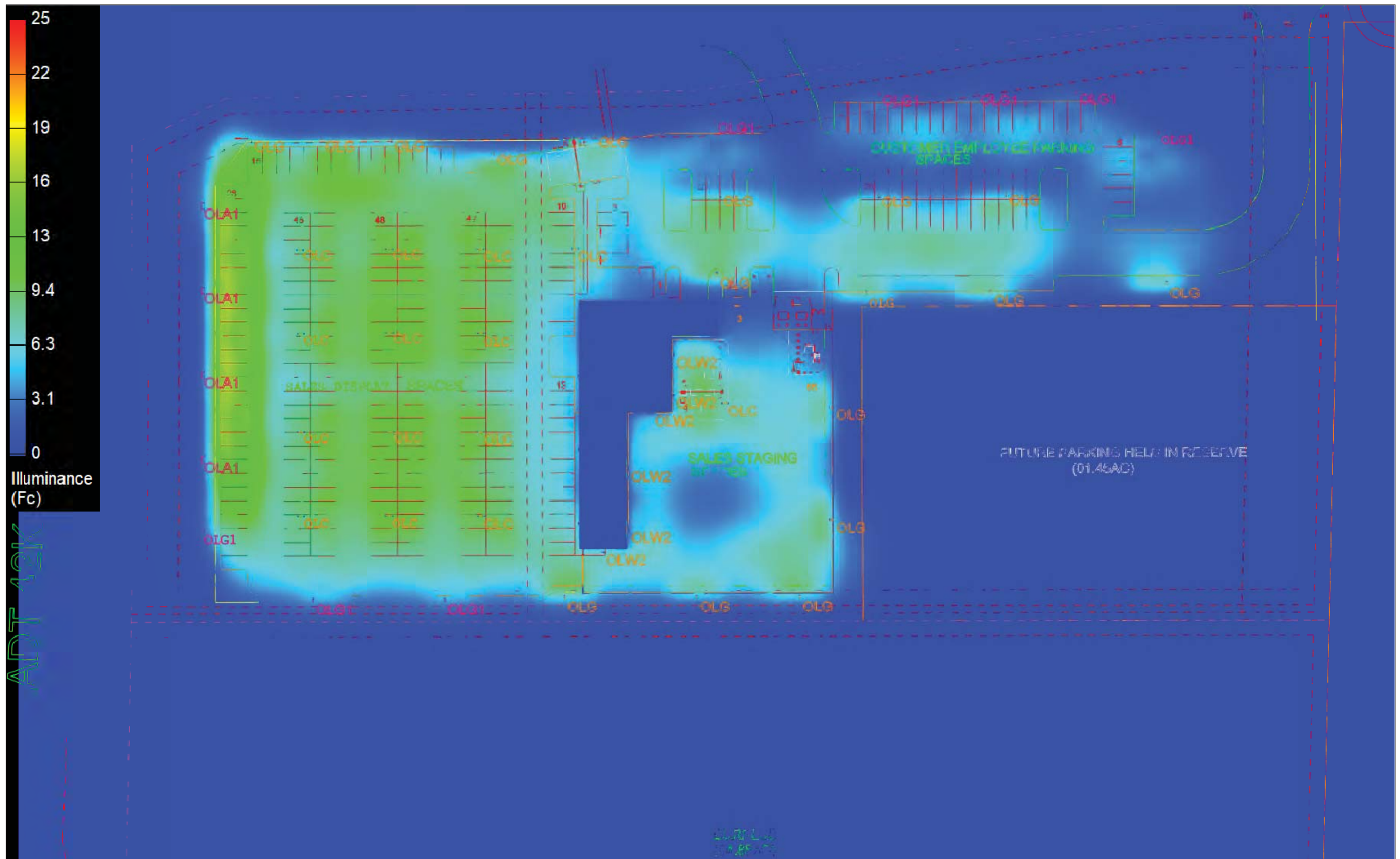
BY: AHK DATE: 2/8/18 REV: 7/18/18 SHEET 1 OF 1

SCALE: 1"=40'

0 40

Based on the information provided, all dimensions and luminaire locations shown represent recommended positions. The engineer and/or architect must determine the applicability of the layout to existing or future field conditions.

This lighting plan represents illumination levels calculated from laboratory data taken under controlled conditions in accordance with The Illuminating Engineering Society (IES) approved methods. Actual performance of any manufacturer's luminaires may vary due to changes in electrical voltage, tolerance in lamps/LED's and other variable field conditions. Calculations do not include obstructions such as buildings, curbs, landscaping, or any other architectural elements unless noted. Fixture nomenclature noted does not include mounting hardware or poles. This drawing is for photometric evaluation purposes only and should not be used as a construction document or as a final document for ordering product.



PSEUDO COLOR RADIOSITY - TOP VIEW - 25FC SCALE
RENDERING SHOWS CALCS 4' ABOVE GRADE
OPERATIONAL HOURS

Based on the information provided, all dimensions and luminaire locations shown represent recommended positions. The engineer and/or architect must determine the applicability of the layout to existing or future field conditions.

This lighting plan represents illumination levels calculated from laboratory data taken under controlled conditions in accordance with The Illuminating Engineering Society (IES) approved methods. Actual performance of any manufacturer's luminaires may vary due to changes in electrical voltage, tolerance in lamps/LED's and other variable field conditions. Calculations do not include obstructions such as buildings, curbs, landscaping, or any other architectural elements unless noted. Picture nomenclature noted does not include mounting hardware or poles. This drawing is for photometric evaluation purposes only and should not be used as a construction document or as a final document for ordering product.

Total Project Watts
Total Watts = 10805.19



LIGHTING PROPOSAL LD-141548-7

CARMAX
ALGONGQUIN, IL
(SP-16)

BY: AHK DATE: 8/8/18 REV: 7/19/19 SHEET 1 OF 1

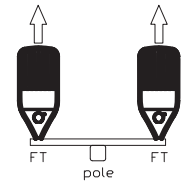
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S RANDALL RD

ADT 42K

N HUNTINGTON DR

AREA TO BE ILLUMINATED

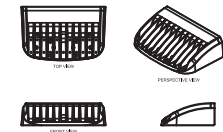


Note: Bolt-on parallel arms are designed for direct mounting of fixture to face of bracket. Standard arm is not required.

XALM
LED Area Light



XWM
LED Wall Mount



ALL 24' POLES TO BE MOUNTED ON CONCRETE PEDESTALS
2' ABOVE FINISHED GRADE FOR A TOTAL MOUNTING HEIGHT OF 26'

NON-OPERATIONAL HOURS
SALES AREA DIMMED 50%

Calculation Summary							
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
CALCS 2' 4' ABOVE GRADE	Illuminance	Fc	1.18	9.5	0.0	N.A.	N.A.
PROPERTY LINE ABOVE OFC @ GRAD	Illuminance	Fc	0.47	0.8	0.0	N.A.	N.A.
PROPERTY LINE BELOW OFC @ GRAD	Illuminance	Fc	0.03	0.3	0.0	N.A.	N.A.
C E LOT	Illuminance	Fc	4.17	9.5	0.1	41.70	95.00
C E LOT WITHOUT DRIVE	Illuminance	Fc	4.27	9.5	0.6	7.12	15.83
SALES LOT	Illuminance	Fc	5.11	9.3	1.9	2.69	4.89
SALES STAGING	Illuminance	Fc	5.53	9.1	1.3	4.25	7.00

LPD Area Summary	
Label	Total Watts
CE LOT	2700.901
SALES	7202.399
SALES STAGING	1733.2
Total Project Watts	11636.49

Luminaire Schedule								
Symbol	Qty	Label	Arrangement	Description	LLF	Lumens/Lamp	Arr. Lum. Lumens	Arr. Watts
	4	OLAI-DIM	TWIN	XALM-2-LED-SS-50-IL-20-TWINKPA-40-SINGLE PREP)	0.500	N.A.	26070	312.4
	1	OLC	D180*	XALM-5W-LED-SS-20	0.900	N.A.	38100	312
	13	OLC-DIM	D180*	XALM-5W-LED-SS-20	0.500	N.A.	38100	312
	12	OLG	SINGLE	XALM-FT-LED-HO-50-IL-20	0.900	N.A.	22020	241.3
	5	OLG-DIM	SINGLE	XALM-FT-LED-HO-50-IL-20	0.500	N.A.	22020	241.3
	5	OLG1	SINGLE	XALM-FT-LED-SS-50-IL-20	0.900	N.A.	14362	154.1
	3	OLG1-DIM	SINGLE	XALM-FT-LED-SS-50-IL-20	0.500	N.A.	14362	154.1
	6	OLW2	SINGLE	XWM-FT-LED-08-50-19' MH	0.900	N.A.	8654	76
	3	OLW2-DIM	SINGLE	XWM-FT-LED-08-50-19' MH	0.500	N.A.	8654	76

Total Project Watts
Total Watts = 11636.49



LIGHTING PROPOSAL LO-141548-7A

CARMAX ALGONGUIN, IL (SP-16)

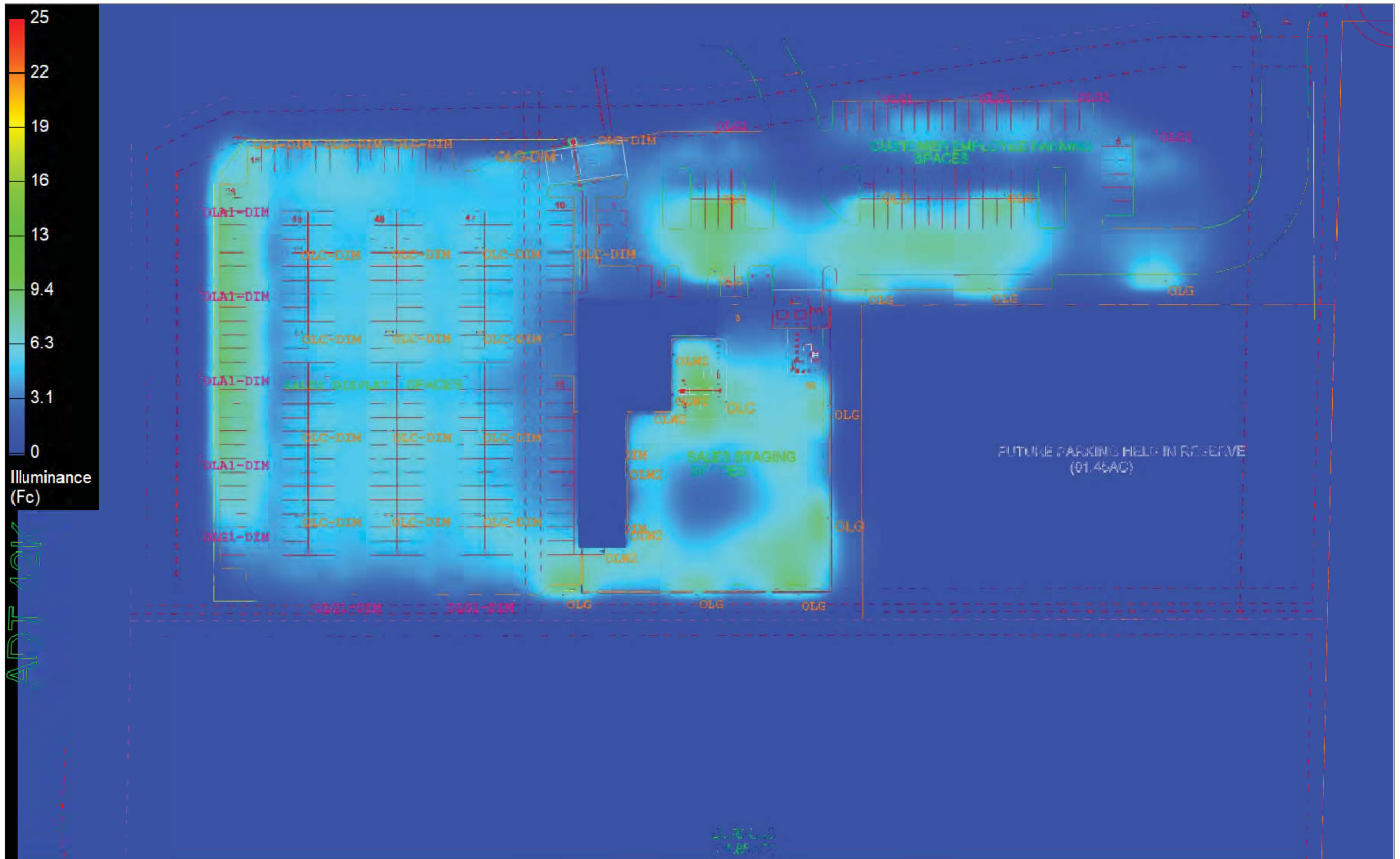
BY: AHK DATE: 2/8/18 REV: 7/19/18 SHEET 1 OF 1

SCALE: 1"=40'

0 40

Based on the information provided, all dimensions and luminaire locations shown represent recommended positions. The engineer and/or architect must determine the applicability of the layout to existing or future field conditions.

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PSEUDO COLOR RADIOSITY - TOP VIEW - 25FC SCALE
RENDERING SHOWS CALCS 4' ABOVE GRADE
NON-OPERATIONAL HOURS (Sales dimmed to 50%)

Based on the information provided, all dimensions and luminaire locations shown represent recommended positions. The engineer and/or architect must determine the applicability of the layout to existing or future field conditions.

This lighting plan represents illumination levels calculated from laboratory data taken under controlled conditions in accordance with The Illuminating Engineering Society (IES) approved methods. Actual performance of any manufacturer's luminaires may vary due to changes in electrical voltage, tolerance in lamps/LED's and other variable field conditions. Calculations do not include obstructions such as buildings, curbs, landscaping, or any other architectural elements unless noted. Fixture nomenclature noted does not include mounting hardware or poles. This drawing is for photometric evaluation purposes only and should not be used as a construction document or as a final document for ordering product.

Total Project Watts
Total Watts = 10805.19



LIGHTING PROPOSAL LD-141548-7A

CARMAX
ALGONGQUIN, IL
(SP-16)

BY: AHK DATE: 8/8/18 REV: 7/19/19 SHEET 1 OF 1

0 40

LED AREA LIGHTS - LSI MIRADA (XALM)



DOE LIGHTING FACTS

Department of Energy has verified representative product test data and results in accordance with its Lighting Facts Program. Visit www.lightingfacts.com for specific catalog strings.

LIGHT OUTPUT - XALM

		Lumens (Nominal) Type 2, Type 5W, Type 3 and Type FT	Watts (Nominal)
3000K	SS	17100	154
	HO	25300	242
	VHO	31700	315
4000K	SS	18300	154
	HO	28000	242
	VHO	33000	315
5000K	SS	18500	154
	HO	29300	242
	VHO	35300	315

LED Chips are frequently updated therefore values may increase.

US & Int'l. patents pending

DISTRIBUTION/PERFORMANCE - Proprietary silicone refractor optics provide exceptional coverage and uniformity in Types 2, 3, 5W and FT. Internal Louver (IL) option available for improved back-light control without sacrificing street side performance.

ENERGY SAVING CONTROL OPTIONS - DIM - 0-10 volt dimming enabled with controls by others. Available with integrated LSI Controls wireless modules.

OCCUPANCY SENSING (IMS) - Optional integral passive infrared motion and daylight sensor activates switching of luminaire light levels. High level light is activated and increased to full bright upon detection of motion. Low light level (30% maximum drive current) is activated when target zone is absent of motion activity for ~2 minutes. Sensor is located on the center of the access cover and has a detection cone of approximately 45°.

LEDS - Select high-brightness LEDs in 5000K, 4000K, and 3000K color temperature, 70 CRI.

HOUSING - Rugged die-cast aluminum housing contains factory prewired driver and optical unit. Cast aluminum wiring access door located underneath. Fixture sealed to IP65.

MOUNTING - Tapered rear design allows fixtures to be mounted in 90° and 120° configurations without the need for extension arms. Use with 3" reduced drilling pattern. Wall mount brackets are available for direct mounting to wall.

ELECTRICAL - Two-stage surge protection (including separate surge protection built into electronic driver) meets IEEE C62.41.2-2002, Location Category C. Available with universal voltage power supply 120-277 VAC (50/60Hz input), and 347-480 VAC. Optional PCR and photocells (PC) are available in 120, 208, 240, 277, 347 and 480 volt (supply voltage must be specified).

DRIVER - Available in SS (Super Saver), HO (High Output) and VHO (Very High Output) drive currents. Components are fully encased in potting material for moisture resistance. Driver complies with FCC standards. Driver and key electronic components can easily be accessed.

OPERATING TEMPERATURE - -40°C to +50°C (-40°F to +122°F)

FINISH - Fixtures are finished with LSI's DuraGrip® polyester powder coat finishing process. The DuraGrip finish withstands extreme weather changes without cracking or peeling. Other standard LSI finishes available. Consult factory.

WARRANTY - LSI LED fixtures carry a limited 5-year warranty.

PHOTOMETRICS - Please visit our web site at www.lsi-industries.com for detailed photometric data.

SHIPPING WEIGHT (in carton) - One fixture: 30 lbs. (13.6 kg).

LISTING - UL listed to U.S. and international safety standards. Suitable for wet locations.

For a list of the specific products in this series that are DLC listed, please consult the LED Lighting section of our website or the Design Lights website at www.designlights.org.

This product, or selected versions of this product, meet the standards listed below. Please consult factory for your specific requirements.



ARRA
Funding Compliant



Fixtures comply with ANSI C136.31-2010 American National Standard for Roadway Lighting Equipment - Luminaire Vibration 1.5G requirements.



Project Name _____

Fixture Type _____

Catalog # _____

6/20/17

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LSI INDUSTRIES INC.



PRELIMINARY DRAINAGE REPORT – CARMAX

Randall Road and Huntington Drive

Algonquin, IL

Prepared by:

Kimley-Horn and Associates, Inc.

1001 Warrenville Road, Suite 350

Lisle, IL 60532

Contact: Michaela Schuering, P.E.

Prepared on: June 22, 2018

Kimley»Horn



TABLE OF CONTENTS

<i>Project Description</i>	<i>02</i>
<i>Detention Design Summary</i>	<i>03</i>

EXHIBITS

- Exhibit 1. FEMA Map Panel, Soil Survey, Wetlands Map*
- Exhibit 2. Grading Plan*
- Exhibit 3. Utility Plan*
- Exhibit 4. Pre-Development HydroCAD Model*
- Exhibit 5. Post-Development HydroCAD Model*

1. PROJECT DESCRIPTION

Kimley-Horn and Associates, Inc., serves as the engineering consultant for Centerpoint Integrated Solutions. The scope of this project is to develop a portion of a 13.53-acre parcel located at the southeast corner of Randall Road and N. Huntington Drive in Algonquin, Illinois, into a proposed CarMax with an approximate 4,160 sf sales building, 2,615 sf service building, sales display lot, and detention pond. Associated site work will include grading, storm sewer installation, sanitary sewer installation, underground service utilities, paving, and landscaping. There are planned future roadway improvements to N. Huntington Drive and Randall Road to be completed by McHenry County Department of Transportation.

This report evaluates the pre and post development runoff characteristics of the development and addresses the stormwater requirements of the Village of Algonquin and McHenry County. The analysis and design of the existing and proposed storm sewer system was completed with the assistance of HydraFlow Storm Sewer Extensions, Version 10 and HydraFlow Express Extension. The analysis of the proposed stormwater detention design was completed with the assistance of HydroCAD Version 10.00 using Huff distributions for the SCS TR-20 method. Additional detailed design calculations will be provided during final engineering for the detention pond and onsite storm sewer system.

1.1. Pre Development Conditions

The existing site is a vacant manufacturing facility. The existing building, parking lot, and decorative pond will be demolished. Additionally, McHenry County roadway improvements will occur beginning summer/fall of 2018 on Huntington Drive and Randall Road. The site currently drains from north to south towards existing wetlands to the south.

Per the FEMA Map Panel, 17111C0337J, dated November 16, 2006, the site is located in Zone X, area of minimal flood hazard. See **Exhibit 1** for the FEMA map.

Per the Natural Resources Conservation Service, the soils are as follows: 22.6% Elliott silt loam, Class C/D, 48.2% Varna silt loam, Class C, and 29.1% Ashkum silty clay loam, Class C/D. The NRCS soils map is located in **Exhibit 1**.

Based on the Wetland Boundary Map provided by PSI Engineering, dated 11/13/2017, there is a wetland located just south of the property. Existing drainage patterns flow toward this wetland. Post-development conditions will follow existing conditions. See **Exhibit 1** for the Wetlands Boundary Map of the site.

1.2. Post Development Conditions

The proposed development is a proposed CarMax facility with an approximate 4,160 square-foot sales building and a 2,615 square-foot service building and sales display lot. Site work includes grading, construction of a detention pond, storm sewer construction, sanitary sewer construction, underground service utilities, paving, and landscaping. Per Village of Algonquin Zoning Map, the site is located in an Industrial Zone. The proposed site will be rezoned to a PD, Planned Development, with underlying B-2, General Retail Business zoning. The site contains a drainage area of

approximately 14.29 acres (including off-site run-on). The stormwater runoff for the proposed site will be conveyed through the storm sewer system to the detention pond located on a dedicated detention parcel east of the site. The pond will release water at a restricted rate into the wetland to the south. The stormwater management system has been designed to meet the Village standard of 0.1 cfs/acre release rate for the 100-year, 24 hour storm, with a total allowable release rate of 1.35 cfs. Along the south property line, a small berm is proposed due to the existing grades onsite. This small portion of the site will drain directly into the offsite regional detention area. A swale is proposed at the top of the berm to collect the surplus lot drainage and drain into the pond.

Refer to **Exhibit 3 and 4** for the Grading Plan and Utility Plan.

2. DETENTION POND DESIGN SUMMARY

2.1. Village of Algonquin Detention Requirements

The proposed development falls within McHenry county, but the Village of Algonquin follows Kane County Stormwater Ordinance. Per Section T203(b) of the Village ordinance, the site may not exceed a release rate of greater than 0.1 cfs/acre of site, or 1.35 cfs. The stormwater storage shall be designed for the 100-year, 24-hour storm. A composite curve number of 86 was used for the pre-development model. The post-development composite curve number was determined to be 83. The site receives approximately 0.79 ac of off-site run-on from the north and west. Per section T1606(f)(5), treatment volume should be a minimum 500 cf/impervious acre, and sediment storage volume should be a minimum 100 cf/impervious acre. The pond provides 0.395 ac-ft of treatment volume and 0.00790 ac-ft of sediment storage below the normal water elevation. The pond will discharge to the proposed constructed wetlands to the south with an outlet control structure with a 4.7" orifice on a weir wall.

2.2. Rate Attenuation Summary

The maximum allowable release rates for the critical 100-year storm, the 48-hour event, was calculated per the Kane County Stormwater Ordinance using HydroCAD Version 10.00. Using a 4.7" orifice in the pond, the proposed peak runoff rate for the 100-year storm is shown below. Detailed calculations have been provided in **Exhibits 5 and 6**.

Table 1: Summary of Maximum Allowable vs. Post Development Runoff Rates	
	<i>100-Year (1%)</i>
Pre Development Release Rate (cfs)	37.05
Post Development allowable release rate (cfs)	1.35
Post Development Release Rate (cfs)	1.33

Per the above table, the proposed detention system meets the rate attenuation standard.

PROPOSED COMMERCIAL REDEVELOPMENT

Traffic Impact Study

Algonquin, Illinois

May 2018

Prepared for:

Centerpoint Integrated Solutions

Kimley»Horn



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EXECUTIVE SUMMARY

Kimley-Horn and Associates, Inc., (Kimley-Horn) has prepared a traffic impact study for a proposed commercial redevelopment project on the southeast quadrant of the intersection of Randall Road (McHenry County Route V29) and Bunker Hill Drive/Huntington Drive in Algonquin, Illinois. The proposed redevelopment would be an automobile sales facility. Access to the site would be provided by two access driveways along Huntington Drive: a full-access driveway aligned opposite an existing commercial driveway (Site Access A) and a right-in only driveway aligned opposite Rolls Drive (Site Access B).

As a part of this traffic impact study, existing and future traffic conditions were evaluated for the signalized intersection of Randall Road/Bunker Hill Drive/Huntington Drive, the minor-leg stop-controlled intersection of Huntington Drive/Rolls Drive, and the proposed site access driveways. The analysis of existing conditions reveals the intersection of Randall Road/Bunker Hill Drive/Huntington Drive currently operates at level of service (LOS) C during the morning peak hour and LOS D during the evening and Saturday midday peak hours. The unsignalized intersections of Huntington Drive/Rolls Drive and Huntington Drive/Private Driveway/Site Access A currently operate with limited delay during each peak hour.

For purposes of this study, a design year of 2023 was assumed. Per direction from McHenry County Division of Transportation (MCDOT), the planned improvements for the Randall Road/Bunker Hill Drive/Huntington Drive, depicted on the Intersection Design Study (IDS) (Appendix), were assumed to be constructed for the Year 2023 future build conditions. The MCDOT improvement project is planned to significantly increase capacity at the signalized intersection. With the planned improvements, the east and west legs would provide dual left-turn lanes, two through lanes, and a dedicated right-turn lane; the north and south legs would provide dual left-turn lanes, three through lanes, and a dedicated right-turn lane. As part of these improvements, Huntington Drive would be widened to provide two eastbound through lanes west of Site Access A. Based on the approved IDS, modifications to the traffic signal would also be implemented in order to provide protected left-turn phasing and a right-turn overlap on each approach.

For the Year 2023 future build condition, improvements to the site access driveways are recommended in order to facilitate site access. At Site Access A, a single inbound lane is recommended in addition to a dedicated left-turn lane and shared through/right-turn lane for outbound traffic. Site Access B should provide a single inbound lane and “Do Not Enter” signage posted to prohibit outbound traffic. At Site Access A, minor-leg stop control should be posted for outbound traffic. Additional details related to the improvements identified above are provided in the *Recommendations & Conclusion* section of this report.

1. INTRODUCTION

Kimley-Horn was retained by Centerpoint Integrated Solutions to perform a traffic impact study for a proposed redevelopment project on the southeast quadrant of the intersection of Randall Road and Bunker Hill Drive/Huntington Drive in Algonquin, Illinois. The site is currently occupied by a vacant office building, which would be removed to accommodate the proposed redevelopment. Existing site access is provided via a full-access driveway to Huntington Drive (Access A) which is aligned opposite an access driveway for the commercial development to the north. An aerial view of the study location and the surrounding roadway network is presented in **Exhibit 1**.

The proposed redevelopment project would include an automobile sales facility consisting of an approximately 7,410 square-foot building for automobile showroom and sales, as well as service uses. Site Access A would remain in its current location to serve the proposed redevelopment project. In addition, a new right-in only driveway from Huntington Drive (Site Access B) would be provided opposite Rolls Drive. Site Access A would provide a single inbound lane and an outbound dedicated left-turn lane and shared through/right-turn lane. Site Access B would provide a single inbound lane.

As a part of this study, the existing network was analyzed to determine current operations at the study intersections, and an evaluation of future traffic conditions was completed for Year 2023. In order to evaluate Year 2023 future build traffic conditions, the future traffic volumes depicted on the IDS approved for the Randall Road/Bunker Hill Drive/Huntington Drive intersection were referenced. Site trip generation characteristics were then established for the proposed automobile sales facility in order to evaluate the impact of the proposed redevelopment. For the future build conditions, the planned improvements depicted on the IDS approved for the Randall Road/Bunker Hill Drive/Huntington Drive intersection were assumed to be constructed per MCDOT direction.

This report presents and documents Kimley-Horn's data collection, summarizes the evaluation of traffic conditions on the surrounding roadways, and identifies recommendations to address the potential impact of site-generated traffic on the adjacent roadway network.



2. EXISTING CONDITIONS

Kimley-Horn conducted a field visit to collect relevant information pertaining to existing land uses in the surrounding area, the adjacent street system, current traffic volumes and operating conditions, lane configurations and traffic controls at nearby intersections, and other key roadway characteristics. The findings of this field investigation are detailed as follows.

2.1. Area Land Uses & Connectivity

Located on the southeast quadrant of the intersection of Randall Road (McHenry County Route V29) and Bunker Hill Drive/Huntington Drive, the subject site is currently occupied by a vacant office building. The site currently has direct access to Huntington Drive.

Near the subject site, parcels fronting Randall Road are occupied by a variety of commercial developments. The southwest quadrant of the intersection of Randall Road and Bunker Hill Drive/Huntington Drive is currently occupied by medical offices. Huntington Drive and Bunker Hill Drive also provide access to commercial developments, including retail and office uses. East of the subject site, Huntington Drive provides access to a single-family residential development. West of Randall Road, Bunker Hill Drive provides access to Harry D. Jacobs High School.

The site is located approximately one-half of a mile south of Algonquin Road, a key east-west thoroughfare which provides access between Illinois Route 31 in Algonquin and Illinois Route 83 in Mount Prospect. In addition, the site is located approximately six miles north of the Jane Addams Memorial Tollway (Interstate 90), with a full-access interchange provided at Randall Road.

2.2. Existing Roadway Characteristics

A field investigation was conducted within the study area along Randall Road, Bunker Hill Drive, and Huntington Drive. The signalized intersection of Randall Road and Bunker Hill Drive/Huntington Drive and the existing and proposed site access driveways were analyzed for this study. Existing characteristics for these study area roadways are summarized below.

Randall Road (McHenry County Route V29) is a north-south roadway that is designated by the Illinois Department of Transportation (IDOT) as a Strategic Regional Arterial (SRA) route. The SRA system was established by IDOT to promote mobility on key routes throughout the Chicago area by applying various strategies, such as access control and limited signalization. Through the study area, Randall Road provides two travel lanes in each direction with dedicated turn lanes provided at key intersections. At its signalized intersection with Bunker Hill Drive/Huntington Drive, Randall Road provides a dedicated left-turn lane, two through lanes, and a dedicated right-turn lane on the north leg. On the south leg of the intersection, Randall Road provides a dedicated left-turn lane, one dedicated through lane, and one shared through/right-turn lane. A crosswalk with pedestrian push buttons for the signal is provided on the south leg of Randall Road at Bunker Hill Drive/Huntington Drive. North of Bunker Hill Drive/Huntington Drive, the posted speed limit along Randall Road is 45 miles per hour (MPH); south of the intersection, the posted speed limit is 50 MPH. Randall Road is under MCDOT jurisdiction.

Bunker Hill Drive is an east-west roadway that extends west of its intersection with Randall Road. Per IDOT, Bunker Hill Drive is designated a major collector roadway and provides a single travel lane in each direction. At its signalized intersection with Randall Road, Bunker Hill Drive provides a dedicated left-turn lane, a single through lane, and a dedicated right-turn lane. A crosswalk with pedestrian push buttons for the signal is provided on Bunker Hill Drive at its intersection with Randall Road. The posted speed limit along Bunker Hill Drive is 30 MPH. Bunker Hill Drive is under Village of Algonquin jurisdiction.

Huntington Drive is an east-west local roadway that runs along the northern boundary of the subject site. Classified by IDOT as a major collector roadway, Huntington Drive provides a single travel lane in each direction. At its intersection with Randall Road, Huntington Drive provides a dedicated left-turn lane and a shared through/right-turn lane. A crosswalk with pedestrian push buttons for the signal is provided on Huntington Drive at its intersection with Randall Road. At its minor-leg stop-controlled intersection with Rolls Drive, Huntington Drive provides a single shared left-turn/through lane on the west leg and a single shared through/right-turn lane on the east leg. A single shared lane is also provided in each direction along Huntington Drive at its intersection with Site Access A. The posted speed limit along Huntington Drive is 30 MPH. Huntington Drive is under Village of Algonquin jurisdiction.

Rolls Drive is a north-south local roadway that extends north from its T-intersection with Huntington Drive. Rolls Drive provides a single travel lane in each direction. At its minor-leg stop-controlled intersection with Huntington Drive, separate dedicated left- and right-turn lanes are provided. A crosswalk is provided on Rolls Drive at its intersection with Huntington Drive. For purposes of this analysis, a 30 MPH speed limit is assumed for Rolls Drive. Rolls Drive is under Village of Algonquin jurisdiction.

Private Driveway is a private access driveway located opposite the existing site access driveway (Site Access A) to Huntington Drive. The driveway provides access to a multi-building commercial development with surface parking lot. A single inbound and single outbound lane are provided at its minor-leg stop-controlled intersection with Huntington Drive. For the purposes of this analysis, a 25 MPH speed limit was assumed for Private Driveway.

The subject property is currently served by a full-access driveway to Huntington Drive, referred to as **Site Access A**. The driveway is aligned opposite Private Driveway and provides a single inbound lane, and outbound left- and shared through/right-turn lanes. Minor-leg stop-control is currently posted for outbound traffic. For the purposes of this analysis, a 25 MPH speed limit was assumed for Site Access A.

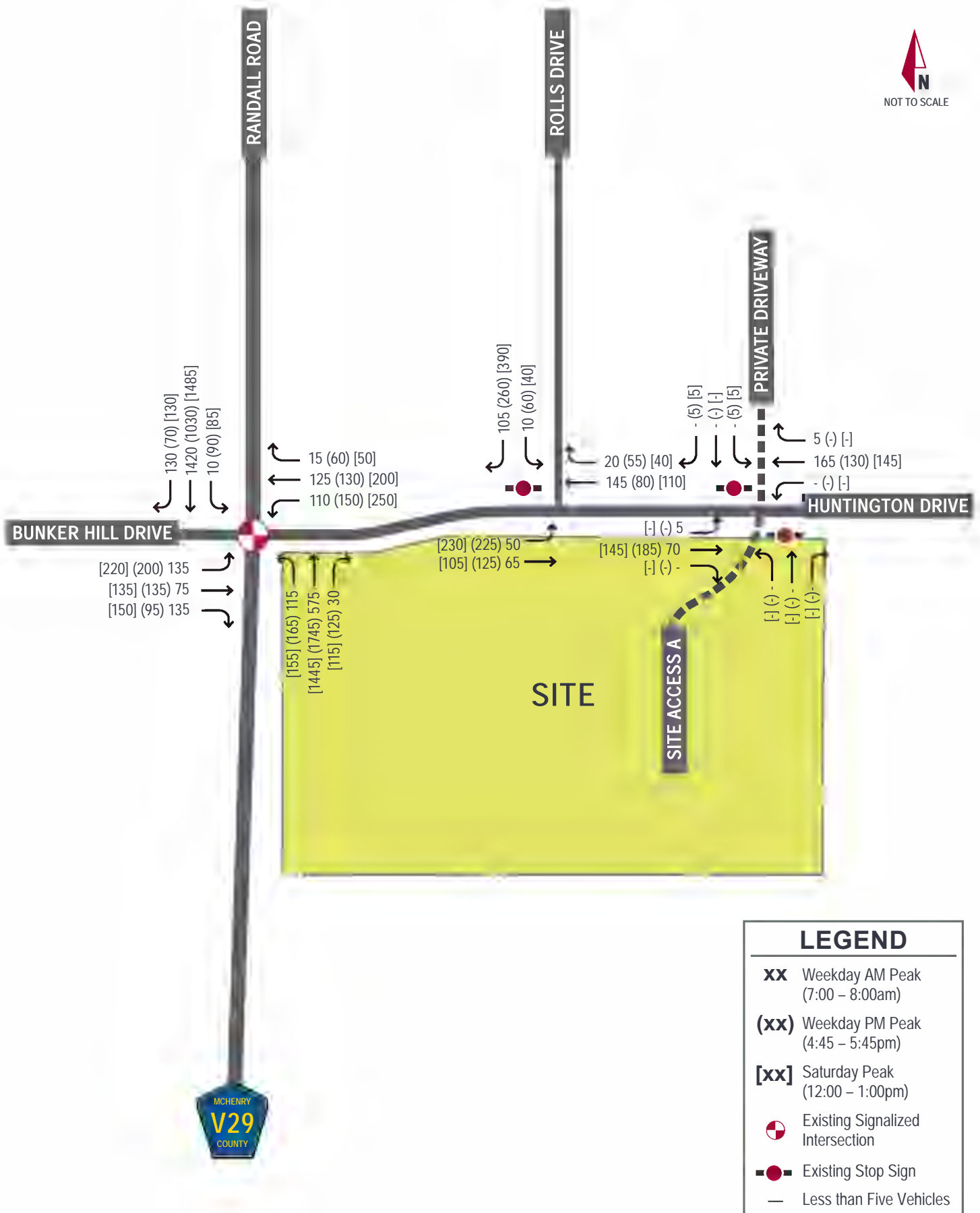
2.3. Traffic Count Data

Turning movement count data was collected on Saturday, January 6, 2018 and Tuesday, January 9, 2018 at the following intersections:

- Randall Road and Bunker Hill Drive/Huntington Drive
- Huntington Drive and Rolls Drive
- Huntington Drive and Private Driveway/Site Access A

Data collection took place during the weekday peak commute periods, from 7:00AM to 9:00AM and 4:00PM to 6:00PM, and during the Saturday midday period, from 11:00AM to 1:00PM. This data indicates that peak traffic volumes occur within the study area on weekdays from 7:00AM to 8:00AM and 4:45PM to 5:45PM and on Saturday from 12:00PM to 1:00PM. Existing peak hour vehicle traffic volumes are presented in **Exhibit 2**.

As shown in the exhibit, existing traffic volumes indicate a weekday commuter pattern with a higher volume of traffic traveling southbound on Randall Road in the morning and northbound in the evening. During the Saturday midday peak hour, traffic volumes along Randall Road are generally more directionally balanced. Traffic along Bunker Hill Drive/Huntington Drive is generally less directional in nature; however, a higher volume of traffic was observed traveling eastbound on Bunker Hill Drive during the evening peak hour as compared to westbound traffic on Huntington Drive during this same period. This may be attributable to traffic exiting Harry D. Jacobs High School, which is located to the west along Bunker Hill Drive. Traffic volumes on Rolls Drive are generally directionally balanced during each peak hour analyzed.



2.4. Existing Capacity Analyses

Synchro capacity software was used to evaluate existing operational conditions at the study intersections during the weekday peak hours. The capacity of an intersection quantifies its ability to accommodate traffic volumes and is expressed in terms of level of service (LOS), measured in average delay per vehicle. LOS grades range from A to F, with LOS A as the highest (best traffic flow and least delay), LOS E as saturated or at-capacity conditions, and LOS F as the lowest (oversaturated conditions). The lowest LOS grade typically accepted by jurisdictional transportation agencies in Northeastern Illinois is LOS D, and a minimum LOS C is required for through movements on SRA routes such as Randall Road.

The LOS grades shown below, which are provided in the Transportation Research Board's Highway Capacity Manual (HCM), quantify and categorize the driver's discomfort, frustration, fuel consumption, and travel times experienced as a result of intersection control and the resulting traffic queuing. A detailed description of each LOS rating can be found in **Table 2.1**.

Table 2.1. Level of Service Grading Descriptions¹

Level of Service	Description
A	Minimal control delay; traffic operates at primarily free-flow conditions; unimpeded movement within traffic stream.
B	Minor control delay at signalized intersections; traffic operates at a fairly unimpeded level with slightly restricted movement within traffic stream.
C	Moderate control delay; movement within traffic stream more restricted than at LOS B; formation of queues contributes to lower average travel speeds.
D	Considerable control delay that may be substantially increased by small increases in flow; average travel speeds continue to decrease.
E	High control delay; average travel speed no more than 33 percent of free flow speed.
F	Extremely high control delay; extensive queuing and high volumes create exceedingly restricted traffic flow.

¹Highway Capacity Manual 2010

The range of control delay for each rating (as detailed in the HCM) is shown in **Table 2.2**. Because signalized intersections are expected to carry a larger volume of vehicles and stopping is required during red time, note that higher delays are tolerated for the corresponding LOS ratings.

Table 2.2. Level of Service Grading Criteria¹

Level of Service	Average Control Delay (s/veh) at:	
	Unsignalized Intersections	Signalized Intersections
A	0 – 10	0 – 10
B	> 10 – 15	> 10 – 20
C	> 15 – 25	> 20 – 35
D	> 25 – 35	> 35 – 55
E	> 35 – 50	> 55 – 80
F ²	> 50	> 80

¹Highway Capacity Manual 2010

²All movements with a Volume to Capacity (v/c) ratio greater than 1 receive a rating of LOS F.

Existing capacity results were identified for the study intersections based on the HCM standards, as summarized in **Table 2.3**. In order to evaluate existing traffic operation, the signal timings for the intersection of Randall Road/Bunker Hill Drive/Huntington Drive were obtained from MCDOT. Right-turn on red (RTOR) movements are not prohibited at the intersection of Randall Road and Bunker Hill Drive/Huntington Drive; and therefore, were included in the capacity analysis. For the signalized intersection of Randall Road/Bunker Hill Drive/Huntington Drive, the capacity results are based on Synchro's Lanes, Volumes, and Timings report. For the minor-leg stop-controlled intersections, the capacity results are based on Synchro's HCM 2010 reports.

In the summary table, operation on each approach is quantified according to the average delay per vehicle and the corresponding level of service. Overall intersection level of service is not reported for minor-leg stop-controlled intersections, since the majority of vehicles are able to move through the intersection with little to no delay.

Table 2.3. Existing (Year 2018) Levels of Service

Intersection	Weekday AM Peak		Weekday PM Peak		Saturday Midday Peak	
	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS
Randall Road / Bunker Hill Drive / Huntington Drive *						
Eastbound	37	D	69	E ³	55	D ²
Westbound	55	D ¹	82	F	102	F
Northbound	21	C ²	58	D ³	50	D ³
Southbound	28	C ²	32	C ³	44	D ³
Intersection	29	C	53	D	54	D
Huntington Drive / Rolls Drive △						
Eastbound (Left)	8	A	8	A	8	A
Southbound	10-	A	12	B	13	B
Huntington Drive / Private Driveway / Site Access A △						
Eastbound (Left)	8	A	8	A	8	A
Westbound (Left)	7	A	8	A	8	A
Northbound	10-	A	11	B	10+	B
Southbound	10+	B	10+	B	10+	B

* – Signalized Intersection

△ – Minor-Leg Stop-Controlled Intersection

¹ Thru movement operates at LOS F.

² Left-turn movement operates at LOS E.

³ Left-turn movement operates at LOS F.

As shown in the preceding table, the intersection of Randall Road/Bunker Hill Drive/Huntington Drive currently operates at LOS C during the morning peak hour and LOS D during the evening and Saturday midday peak hours. The northbound and southbound approaches operate at LOS D or better during each peak hour. The eastbound approach operates at LOS D during the weekday morning and Saturday midday peak hours, and at LOS E during the weekday evening peak hour. The westbound approach operates at LOS D during the weekday morning peak hour and LOS F during the weekday evening and Saturday midday peak hours. Existing signal timings are designed to promote through traffic on Randall Road, resulting in a relatively small quantity of green time for the east and west legs of the intersection. With long cycle lengths currently in place at this intersection (130 seconds in the morning peak hour, 160 seconds in the evening and Saturday midday peak hours), vehicles on the minor legs may be required to wait during a red indication that lasts approximately 80 seconds or more. Since the minimum delay threshold for LOS E is 55 seconds, it is not surprising that the average delay for eastbound and westbound vehicles represents an LOS E or LOS F rating during some peak hours.

The unsignalized intersections of Huntington Drive/Rolls Drive and Huntington Drive/Private Driveway/Site Access A currently operate with limited delay during each peak hour. During each peak hour analyzed, the 95th percentile queues at the unsignalized intersections are estimated to be approximately one vehicle or less for each approach or movement.

Consistent with MCDOT requirements, a summary of the 95th percentile queues estimated for the intersection of Randall Road/Bunker Hill Drive/Huntington Drive is provided in **Table 2.4**. As the estimated 95th percentile queues are one vehicle or less, the unsignalized intersections have been excluded from the table.

Table 2.4. Existing (Year 2018) 95th Percentile Queues - Randall Road/Bunker Hill Drive/Huntington Drive

Intersection	Length of Storage Lane (Feet)	95 th Percentile Queue (Feet)		
		Weekday AM Peak	Weekday PM Peak	Saturday Midday Peak
Randall Road / Bunker Hill Drive / Huntington Drive *				
Eastbound				
Left-Turn Movement	75	153	309	320
Thru Movement	75 ¹	107	212	207
Right-Turn Movement	75	60	43	118
Westbound				
Left-Turn Movement	105	126	208	347
Thru/Right-Turn Movements	105 ¹	181	339	504
Northbound				
Left-Turn Movement	400	158	253	265
Thru/Right-Turn Movements	400 ¹	200	1283	973
Southbound				
Left-Turn Movement	210	30	177	162
Thru Movement	185 ¹	778	514	893
Right-Turn Movement	185	42	0	43

* – Signalized Intersection

■ Indicates the estimated 95th percentile queue exceeds the storage length currently provided in the dedicated turn lane.

¹ Represents distance of through lane from the stop bar to the back of the dedicated left- or right-turn storage lane, whichever distance is less.

As shown in Table 2.4, in some cases the estimated 95th percentile queues exceed the available turn lane storage length during the peak hours. Queue spillback from the dedicated turn lanes to the through travel lanes was observed during peak hour field observations. During field observations, in some instances queues in the through travel lanes on the eastbound and westbound approaches would block access to the dedicated turn lanes.

The 95th percentile queues estimated for the northbound left-turn movement and the southbound left- and right-turn movements are accommodated within the existing storage lanes. This was confirmed through field observations; queue spillback to the through travel lanes on Randall Road was not observed during the peak hours analyzed.

3. FUTURE CONDITIONS

This section of the report outlines the proposed site plan, summarizes site-specific traffic characteristics, develops future traffic projections for analysis, and presents the results of the future capacity analysis.

3.1. Development Characteristics & Site Access

The proposed redevelopment project would include an automobile sales facility consisting of an approximately 7,410 square-foot building for automobile showroom and sales, as well as service uses. In order to accommodate the proposed redevelopment, the existing vacant office building would be removed.

The proposed redevelopment would be accessed via Site Access A, an existing full-access driveway to Huntington Drive (aligned opposite Private Driveway). At this driveway, a single inbound lane and an outbound dedicated left-turn lane and shared through/right-turn lane are provided. In addition, Site Access B, a right-in only driveway from Huntington Drive, would be aligned opposite Rolls Drive. Site Access B would provide a single inbound lane.

3.2. Trip Generation

In order to calculate site-generated traffic projections for the proposed development, data was referenced from the Institute of Transportation Engineers (ITE) manual titled Trip Generation, Tenth Edition. These trip generation rates are shown in **Table 3.1**, and copies of the ITE data are provided in the Appendix.

Table 3.1. ITE Trip Generation Data by Land Use

ITE Land Use	Unit	Weekday			Saturday Midday Peak
		Daily	AM Peak	PM Peak	
Automobile Sales (New) (LUC 840)	Per 1,000 sq. ft.	$T = 28.65(X) - 29.45$ 50% in/50% out	1.87 73% in/27% out	$T = 1.80(X) + 21.60$ 40% in/60% out	4.02 ¹ 50% in/50% out

T = Projected number of trips X = 1,000 square feet of density

¹ For the Saturday Peak Hour of Generator (Saturday Peak Hour of Adjacent Street Traffic not provided), the nature of the ITE equation subtracts approximately 95 trips regardless of the size of the automobile sales facility. As such, the average rate was used to estimate Saturday midday peak hour trips for the proposed automobile sales facility. If the ITE equation ($\ln(T) = 8.56(X) - 95.19$) was used, the Saturday midday peak hour trip generation would be reduced to a negative number. Use of the average rate results in an estimate of 30 total trips (15 inbound, 15 outbound) for the automobile sales facility during the Saturday midday peak hour.

Per these assumptions and the calculations detailed previously, site-generated traffic projections are presented in **Table 3.2** on the following page.

Table 3.2. Site-Generated Traffic Projections

Land Use	Unit	Daily	Weekday						Saturday		
			AM Peak			PM Peak			Midday Peak		
			In	Out	Total	In	Out	Total	In	Out	Total
Automobile Sales (LUC 840)	7,480 sq. ft.	180	10	5	15	15	20	35	15	15	30
<i>Total Trips</i>		<i>180</i>	<i>10</i>	<i>5</i>	<i>15</i>	<i>15</i>	<i>20</i>	<i>35</i>	<i>15</i>	<i>15</i>	<i>30</i>

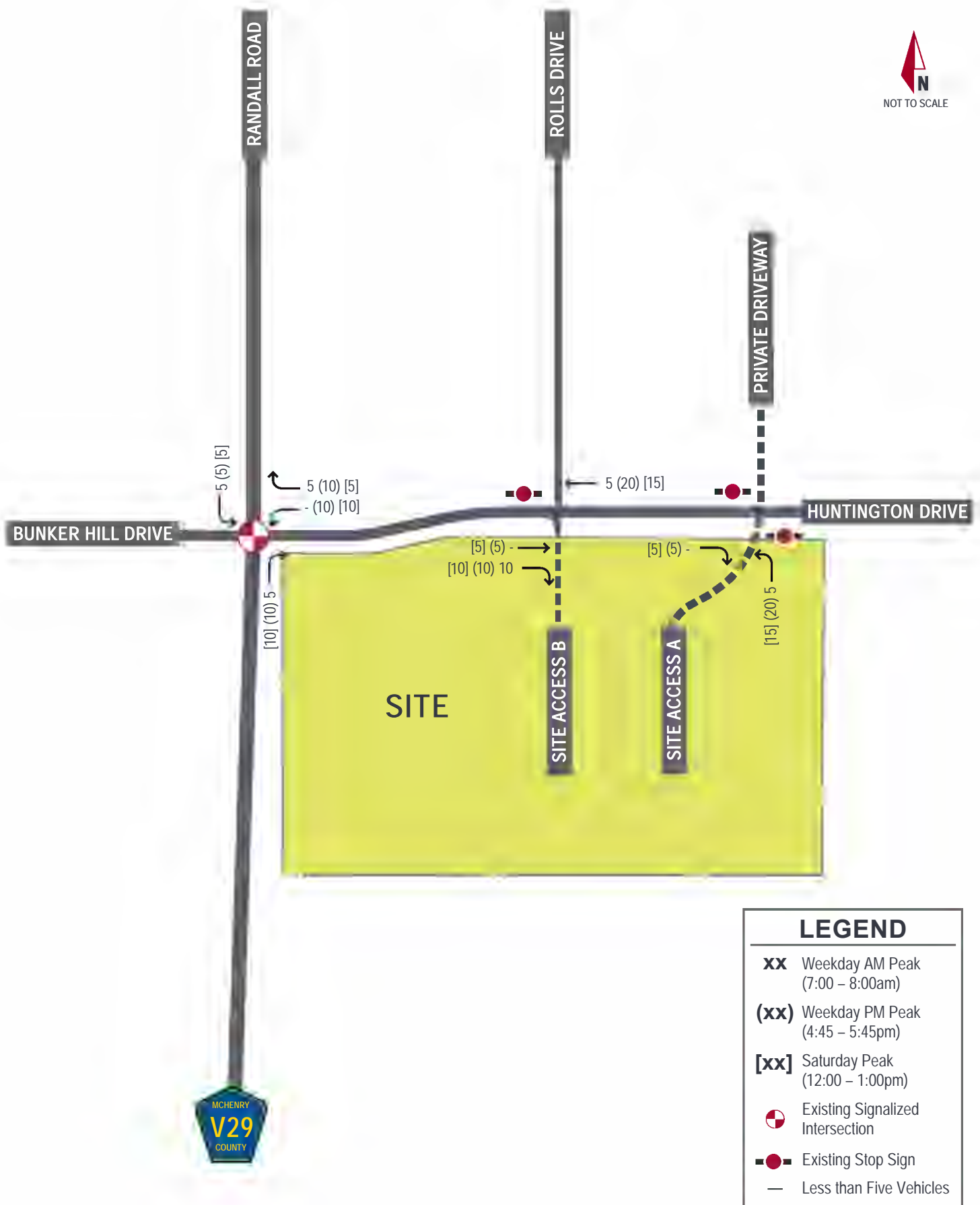
The estimated distribution of site-generated traffic on the surrounding roadway network as it approaches and departs the site is a function of several variables, such as the nature of surrounding land uses, prevailing traffic volumes/patterns, characteristics of the street system, and the ease with which motorists can travel over various sections of that system. The anticipated directional distribution for trips is outlined in **Table 3.3** below.

Traffic generated by the proposed automobile sales facility is expected to primarily utilize Randall Road to access the site access driveways along Huntington Drive. On average, traffic patterns from the north and south along Randall Road are generally evenly distributed, which is reflected in the anticipated trip distribution.

Table 3.3. Estimated Trip Distribution

Traveling to/from:	Estimated Trip Distribution
North via Randall Road	50%
South via Randall Road	50%
West via Bunker Hill Drive / Huntington Drive	0%
East via Bunker Hill Drive	0%
Total	100%

Based on the preceding trip distribution assumptions, site trip assignments are depicted in **Exhibit 3**.



3.3. Future Capacity Analysis

Kimley-Horn evaluated Year 2023 build conditions in order to measure the impact of site-generated traffic with background traffic growth considered.

Background Traffic Growth

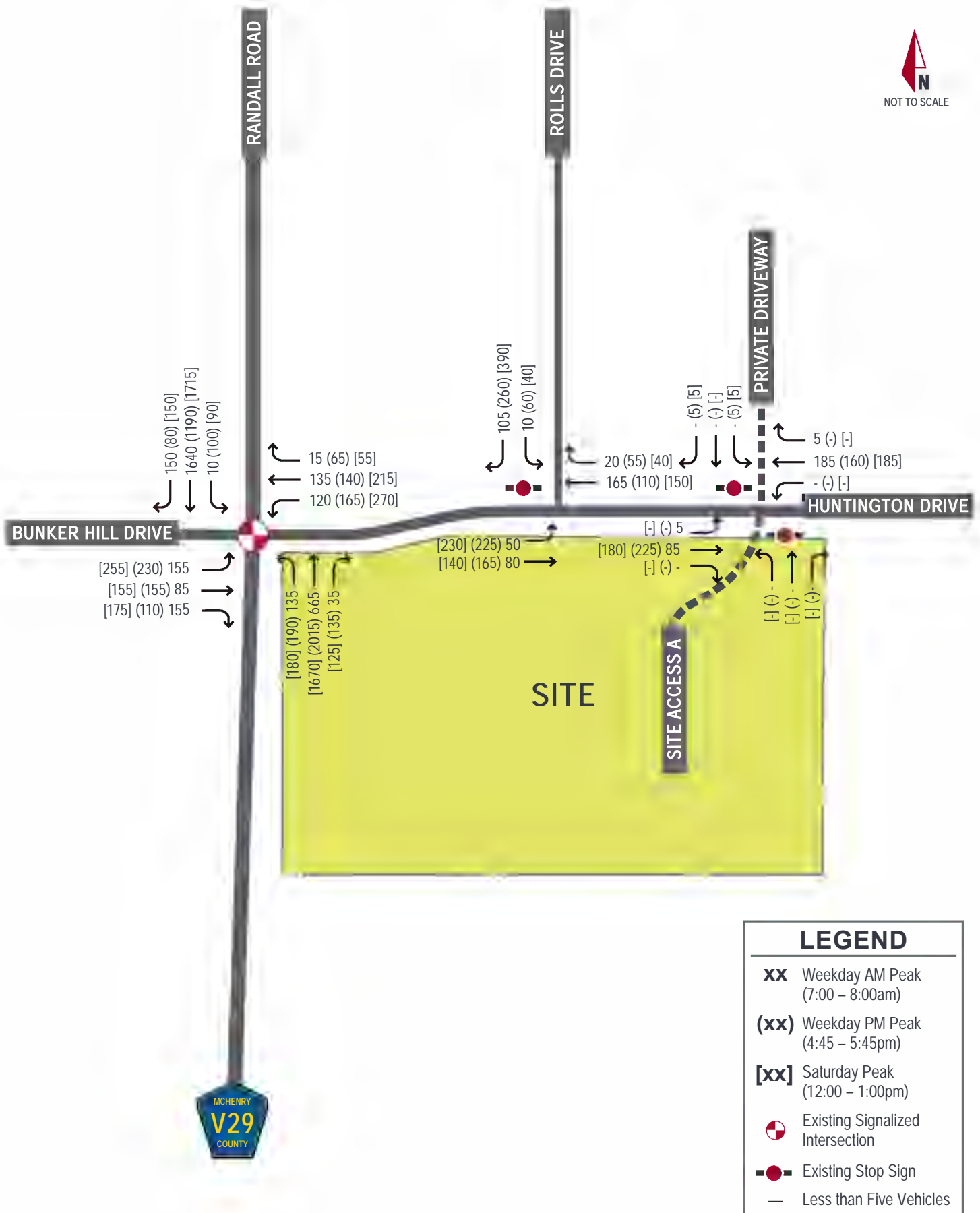
Future traffic projections for Year 2023 were developed based on data provided on the IDS prepared for the Randall Road/Bunker Hill Drive/Huntington Drive intersection. Per the IDS, an annual growth rate of 1.62 percent was applied to westbound and eastbound traffic volumes on Huntington Drive, and an annual growth rate of 2.93 was applied to northbound and southbound volumes on Randall Road, as well as eastbound and westbound volumes on Bunker Hill Drive for the weekday evening peak hour. For the weekday morning, weekday evening, and Saturday midday peak hours, the traffic growth rates identified on the IDS for the weekday evening peak hour were applied to existing traffic count data collected for all three peak hours (Exhibit 2) at the intersection of Randall Road/Bunker Hill Drive/Huntington Drive and distributed through the Huntington Drive corridor to estimate traffic volumes at its intersection with Site Access A and Site Access B.

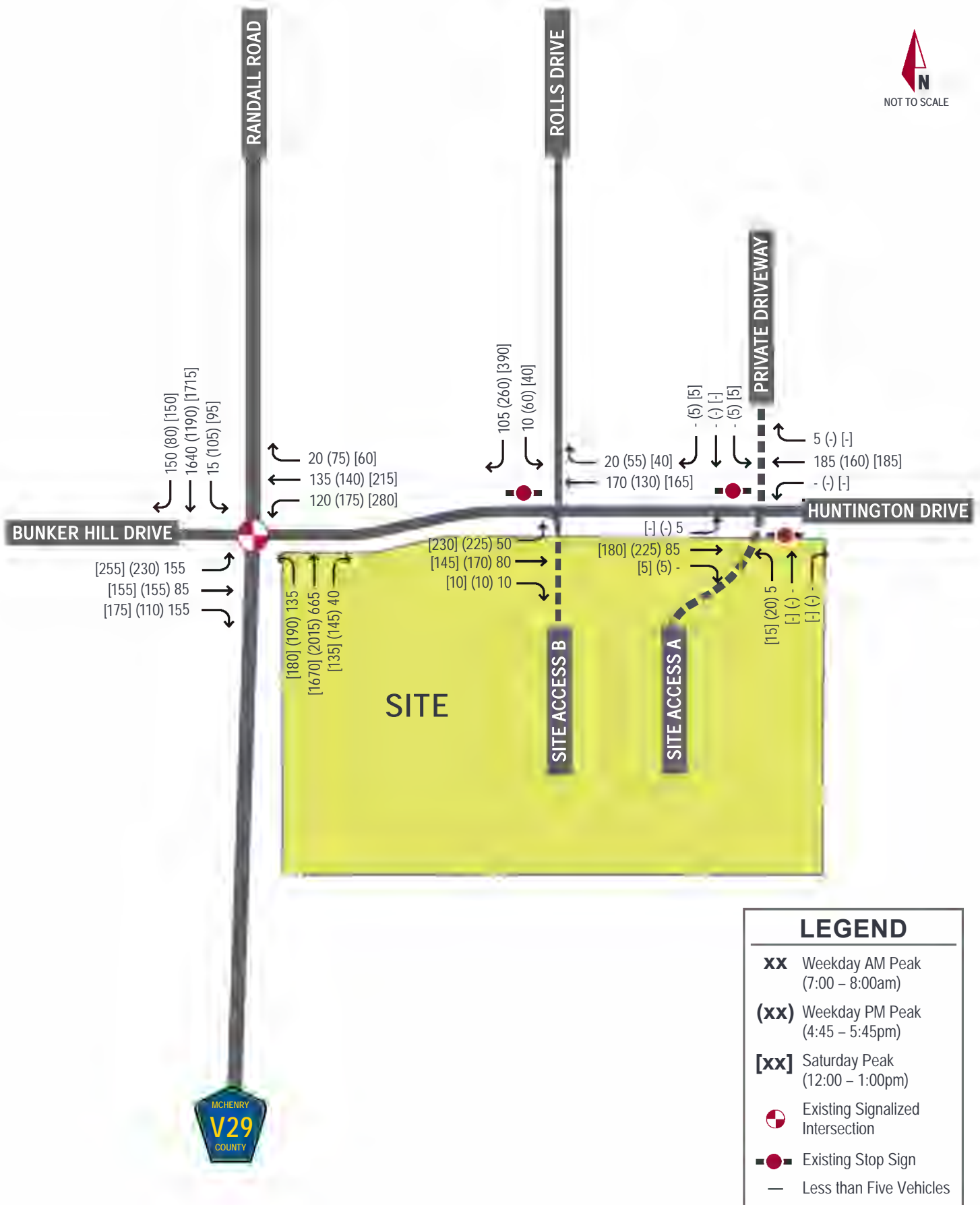
Rolls Drive, Private Driveway, and Site Access A were assumed to experience no background traffic growth. Property fronting Rolls Drive is currently developed, and therefore, limited traffic growth is expected along this local roadway. Similarly, Private Driveway is expected to experience limited traffic growth through Year 2023. The resulting background traffic projections for Year 2023 are presented in **Exhibit 4**.

Future Build Traffic Projections

Traffic projections for the Year 2023 build condition were calculated by adding site trips estimated for the development (Exhibit 3) to Year 2023 background traffic volumes (Exhibit 4). Traffic projections for the Year 2023 future build conditions are illustrated in **Exhibit 5**.

For the analysis of the Randall Road/Bunker Hill Drive/Huntington Drive intersection, the infrastructure improvements shown on the approved IDS (Appendix) were assumed for Year 2023 future build conditions. With the planned improvements, the east and west legs would provide dual left-turn lanes, two through lanes, and a dedicated right-turn lane; the north and south legs would provide dual left-turn lanes, three through lanes, and a dedicated right-turn lane. Additionally, Huntington Drive would be widened to provide two eastbound through lanes west of Site Access A. Based on the approved IDS, modifications to the traffic signal would also be implemented in order to provide protected left-turn phasing and a right-turn overlap on each approach. For purposes of the capacity analysis, the cycle lengths and splits provided on the approved IDS were assumed for the weekday evening peak hour. For the weekday morning and Saturday midday peak hours, the cycle lengths and splits were optimized for future traffic conditions reflected in this study.





Turn Lane Warrants

For the analysis of future traffic conditions, turn lane warrants were evaluated at the proposed site access driveways using guidelines in the McHenry County Access Control and Right-of-Way Management Ordinance. Huntington Drive is a local roadway under the jurisdiction of the Village of Algonquin, which typically defaults to MCDOT standards to evaluate whether a new turn lane should be added at a study intersection. Copies of the MCDOT turn-lane warrant criteria are provided in the Appendix.

Auxiliary turn lane requirements were evaluated for Huntington Drive at Site Access A and Site Access B. Based on the volume criteria outlined in the McHenry County Access Control and Right-of-Way Management Ordinance, where a roadway carries 100-400 vehicles in one direction (similar to eastbound Huntington Drive east and west of Rolls Drive), a minimum right-turn volume of approximately 30-60 vehicles is necessary to warrant a right-turn lane. As depicted in Exhibit 3, the proposed automobile sales facility is expected to generate limited eastbound right-turning traffic on Huntington Drive at Site Access A and Site Access B; and therefore, dedicated right-turn lanes are not warranted. Accordingly, dedicated right-turn lanes were excluded from the analysis of Year 2023 build conditions.

A dedicated westbound left-turn lane was also evaluated for Huntington Drive at Site Access A. Per MCDOT requirements, a minimum left-turn volume of approximately 15 vehicles is necessary to warrant a left-turn lane where a roadway carries 150 to 200 vehicles in one direction during the peak hour (similar to westbound Huntington Drive east of Site Access A). Site-generated traffic estimated for the automobile sales facility is not expected to meet the left-turn lane warrant criteria; and therefore, the turn lane was excluded from the analysis of Year 2023 future build conditions.

Based on these recommendations and assumptions, capacity analysis was performed for Year 2023 build conditions, yielding the results shown in **Table 3.4**. A summary of the projected 95th percentile queues for the Year 2023 build condition is provided in **Table 3.5**. Similar to the existing capacity analysis (Section 2.4), for the signalized intersection of Randall Road/Bunker Hill Drive/Huntington Drive, the capacity results are based on Synchro's Lanes, Volumes, and Timings report. For the minor-leg stop-controlled intersections, the capacity results are based on Synchro's HCM 2010 reports.

Table 3.4. Year 2023 Future Build Levels of Service

Intersection	Weekday AM Peak		Weekday PM Peak		Saturday Midday Peak	
	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS
Randall Road / Bunker Hill Drive / Huntington Drive *						
Eastbound	58	E	60	E	59	E
Westbound	63	E	69	E ²	62	E
Northbound	20	B ¹	25	C ¹	25	C ¹
Southbound	19	B ¹	22	C ¹	26	C ¹
Intersection	27	C	31	C	34	C
Huntington Drive / Rolls Drive / Site Access B △						
Eastbound (Left)	8	A	8	A	8	A
Southbound	10+	B	13	B	14	B
Huntington Drive / Private Driveway / Site Access A △						
Eastbound (Left)	8	A	8	A	8	A
Westbound (Left)	7	A	8	A	8	A
Northbound	10+	B	12	B	12	B
Southbound	10+	B	10+	B	10+	B

* – Signalized Intersection

△ – Minor-Leg Stop-Controlled Intersection

¹ Left-turn movement operates at LOS E.

² Left-turn movement operates at LOS F.

As shown in the table above, the signalized intersection of Randall Road/Bunker Hill Drive/Huntington Drive is projected to operate at LOS C during each peak hour analyzed. The delay projected for the overall intersection represents a decrease as compared to the existing conditions presented in Table 2.3. The decrease in overall intersection delay is due to the planned intersection improvements depicted in the approved IDS (Appendix). The delay projected for the minor leg approaches also represents a decrease as compared to existing conditions. Per the McHenry County Access Control and Right-of-Way Management Ordinance, the LOS letter grade shall not decrease at signalized intersections adjacent to a proposed development, and the volume-to-capacity (v/c) ratio shall not significantly degrade as a result of site-generated traffic. As shown in Table 3.4 above, with the addition of site-generated traffic and planned intersection improvements, delay is expected to decrease for the overall intersection during each peak hour. A review of the capacity reports provided in the Appendix confirms the v/c ratio is not expected to significantly degrade with the addition site-generated traffic and planned intersection improvements. Based on these results, mitigation is not required for the intersection of Randall Road/Bunker Hill Drive/Huntington Drive.

The Huntington Drive intersections with Rolls Drive/Site Access B and Private Driveway/Site Access A are expected to operate with limited delay at LOS B or better for each turn movement or approach. Based on the projected delay at the site access driveways, mitigation is not required for these intersections.

A summary of the 95th percentile queues estimated for the study intersections is provided in **Table 3.5** on the following page. The 95th percentile queues at the site access driveways are expected to be limited. Overall, the projected 95th percentile queues are not expected to materially impact operations on the adjacent street network or the site access driveways.

Table 3.5. Year 2023 95th Percentile Queues

Intersection	Length of Storage Lane (Feet)	95 th Percentile Queue (Feet)		
		Weekday AM Peak	Weekday PM Peak	Saturday Middy Peak
Randall Road / Bunker Hill Drive / Huntington Drive *				
Eastbound				
Left-Turn Movement	355	107	153	159
Thru Movement	350 ¹	65	103	107
Right-Turn Movement	350	188	126	204
Westbound				
Left-Turn Movement	235	87	142	173
Thru Movement	125 ¹	97	99	141
Right-Turn Movement	125	40	102	82
Northbound				
Left-Turn Movement	455	96	126	120
Thru Movement	250 ¹	133	570	486
Right-Turn Movement	250	21	79	65
Southbound				
Left-Turn Movement	380	20	79	73
Thru Movement	215 ¹	440	299	542
Right-Turn Movement	215	74	47	84

* – Signalized Intersection △ – Minor-Leg Stop-Controlled Intersection

■ Indicates the estimated 95th percentile queue exceeds the storage length currently provided in the dedicated turn lane.

¹ Represents distance of through lane from the stop bar to the back of the dedicated left- or right-turn storage lane, whichever distance is less.

Table 3.5. Year 2023 95th Percentile Queues (continued)

Intersection	Length of Storage Lane (Feet)	95 th Percentile Queue (Feet)		
		Weekday AM Peak	Weekday PM Peak	Saturday Middy Peak
Huntington Drive / Rolls Drive / Site Access B △				
Eastbound Left-Turn Movement	390 ¹	25 ²	25 ²	25 ²
Southbound				
Left-Turn Movement	N/A ³	25 ²	25 ²	25 ²
Right-Turn Movement	N/A ³	25 ²	33	68
Huntington Drive / Private Driveway / Site Access A △				
Eastbound Left-Turn Movement	225 ⁴	-- 5	-- 5	-- 5
Westbound Left-Turn Movement	900 ⁶	-- 5	-- 5	-- 5
Northbound Left-Turn Movement	N/A ⁷	-- 5	25 ²	25 ²
Northbound Right-Turn Movement	N/A ⁷	-- 5	-- 5	-- 5
Southbound Approach	N/A ⁷	-- 5	-- 5	25 ²

★ – Signalized Intersection △ – Minor-Leg Stop-Controlled Intersection

■ Indicates the estimated 95th percentile queue exceeds the storage length currently provided in the dedicated turn lane.

¹ Dedicated left-turn storage lane not provided. Distance between Rolls Drive/Site Access B and Randall Road is approximately 390 feet (measured from the westbound stop bar on Huntington Drive to the start of the radius return for Rolls Drive).

² Per Synchro's HCM 2010 reports, the 95th percentile queues at unsignalized intersections are reported as number of vehicles. The projected 95th percentile queues for the intersection approach(es) and individual turning movements are approximately one vehicle or less. For the purposes of this analysis, a passenger vehicle is assumed to be 25 feet in length; therefore, the projected 95th percentile queues are approximately 25 feet or less.

³ Striped turn lanes not provided. Distance between Huntington Drive and commercial access driveway on the west side of Rolls Drive is approximately 240 feet (measured from the southbound stop bar on Rolls Drive and to the start of the radius return for the commercial driveway).

⁴ Dedicated left-turn storage lane not provided. Distance between Private Driveway/Site Access A and Rolls Drive is approximately 225 feet (measured from the start of the radius return for Rolls Drive and the start of the radius return for Site Access A).

⁵ Per Synchro's HCM 2010 reports, the 95th percentile queues at unsignalized intersections are reported as number of vehicles. The projected 95th percentile queues for the intersection approach(es) and individual turning movements are reported as zero (0).

⁶ Dedicated left-turn storage lane not provided. Distance between Private Driveway/Site Access A and Stonegate Road is approximately 900 feet (measured from the start of the radius return for Stonegate Road and the start of the radius return for Site Access A).

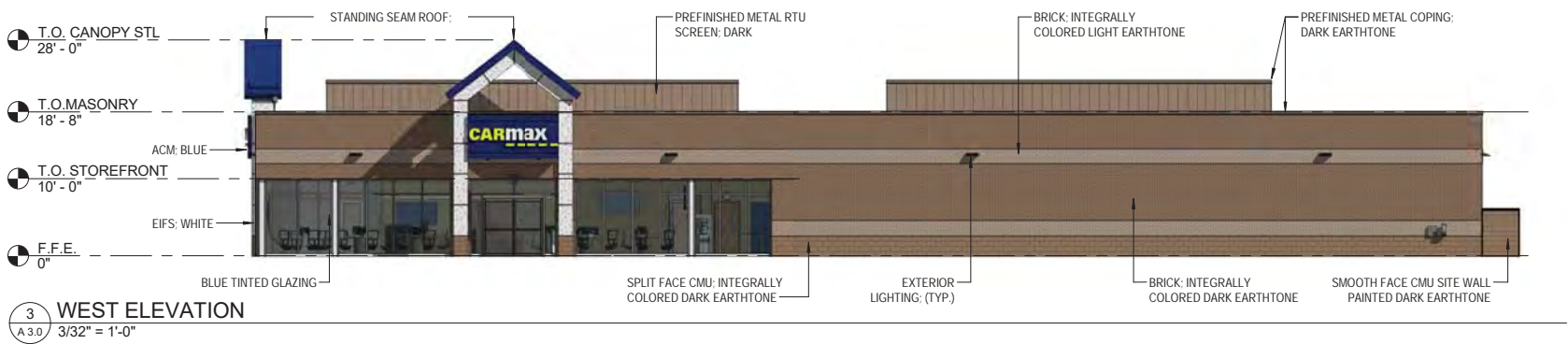
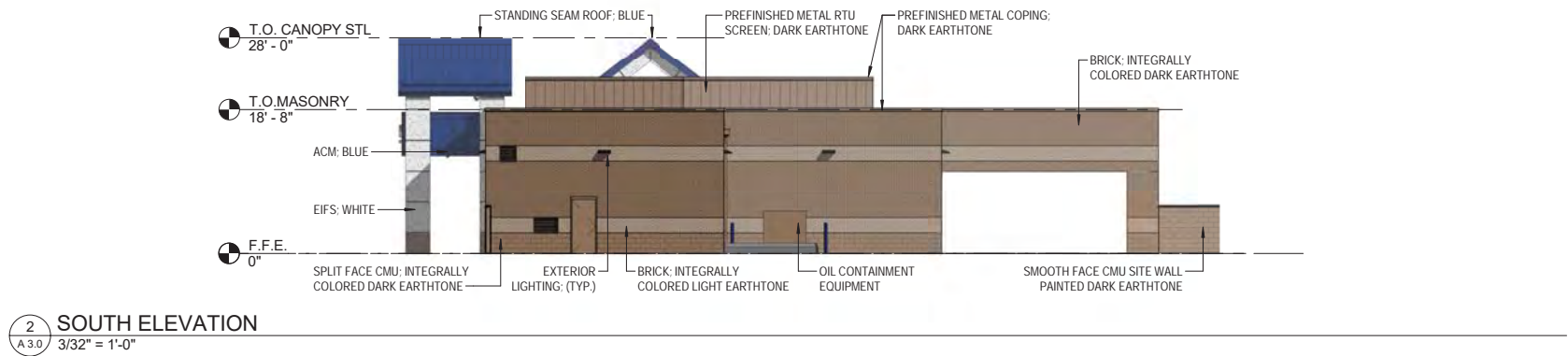
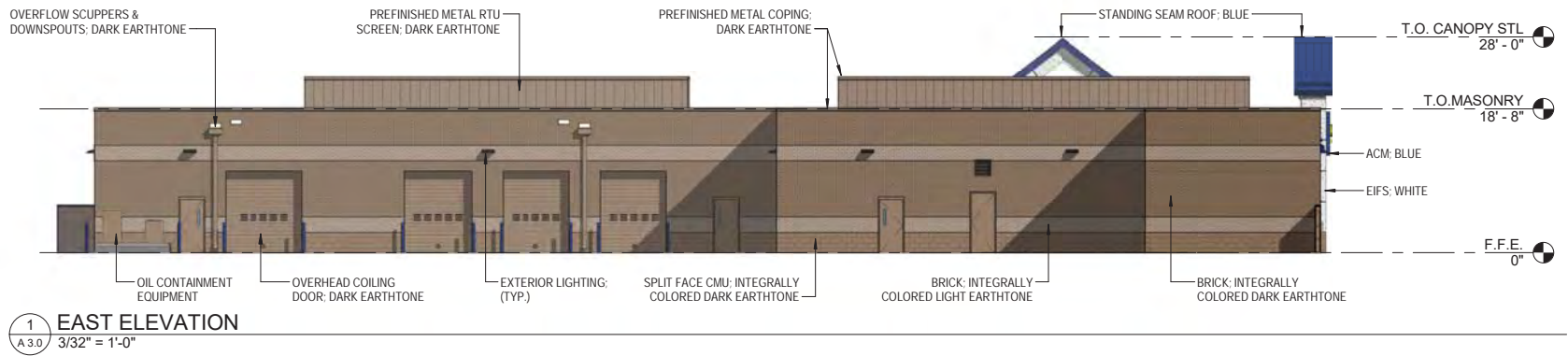
⁷ Site access driveway; vehicle storage for exiting vehicles is provided onsite.

4. RECOMMENDATIONS & CONCLUSIONS

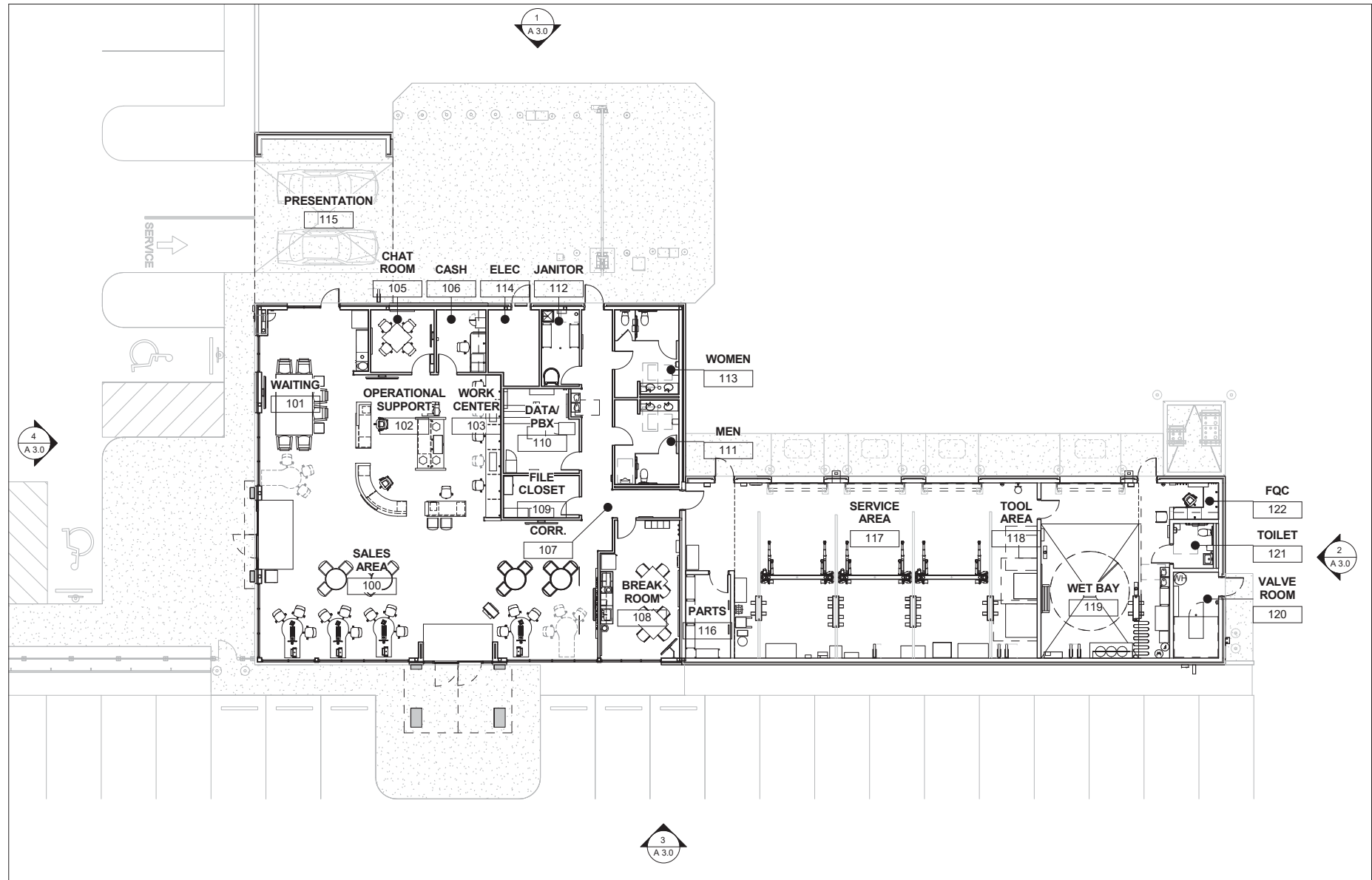
Based on the analysis of Year 2023 future build conditions, several improvements were identified to facilitate site access and mitigate traffic generated by the proposed redevelopment. A summary of the recommended improvements is provided below.

- Huntington Drive / Private Driveway / Site Access A
 - Provide a single inbound lane and a dedicated left-turn lane and shared through/right-turn lane for outbound traffic at Site Access A.
 - Post minor-leg stop control for outbound traffic.
- Huntington Drive / Rolls Drive / Site Access B
 - Provide a single inbound lane at Site Access B.
 - Install “Do Not Enter” signage to prohibit outbound traffic.

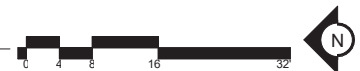
Regardless of the final configuration of the intersection geometrics, several additional items should be taken into consideration when preparing site and roadway improvement plans for the subject development. While vertical sight distance appears to be adequate within the study area, care should be taken with landscaping, signage, and monumentation at the site access locations to ensure that adequate horizontal sight distance is provided from the new stop bar. If alterations to the site plan or land use should occur, changes to the analysis provided within this traffic impact study may be needed.







1 FLOOR PLAN
A 2.0 3/32" = 1'-0"





ELEVATIONS:

PAD: 15'-6"W X 3'-6"D X 6"T
FOOTING: 5'W X 4'9"D X 3'T

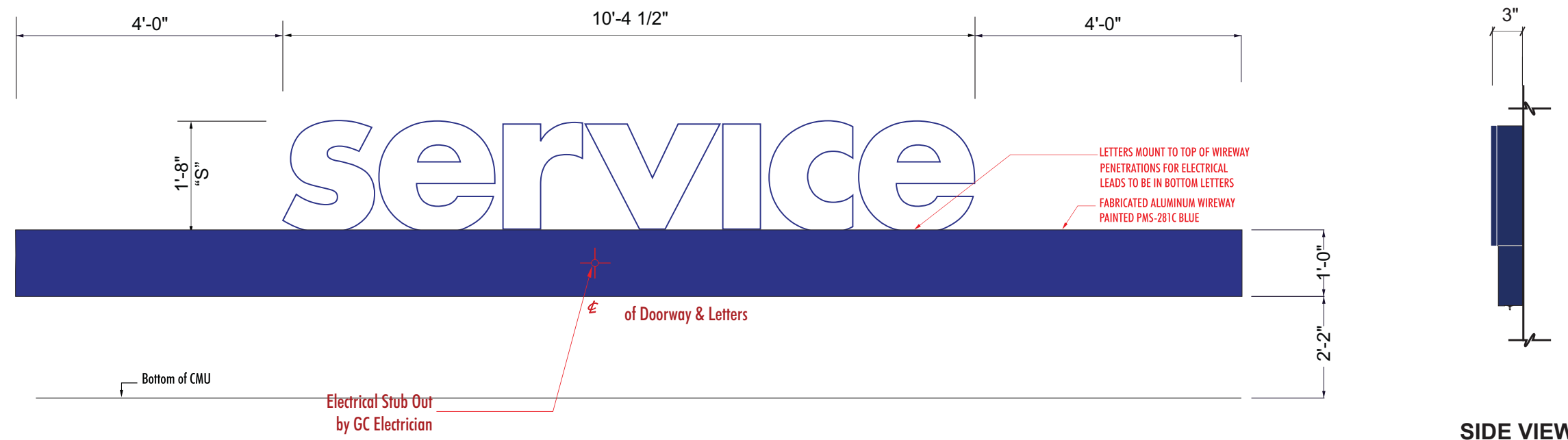
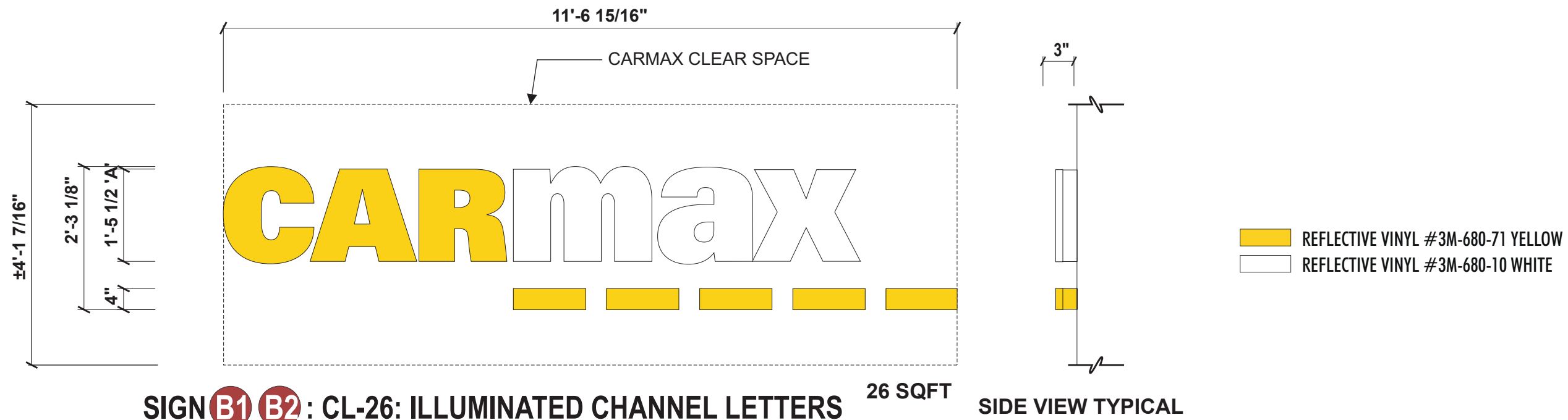
KMX-M50-50-8 - 50 SQFT

SIDE VIEW

Revisions:	
1	02/02/2018
2	04/05/2018
3	04/27/2018 ca
4	05/01/2018 ca
5	
6	
Account Rep:	
Project Manager: M.RHODES	
Drawn By: JER	
Project / Location:	



CARMAX # 6125
Site III Randall Rd
Algonquin, IL 60102



- GRIP-GARD SATIN FINISH CARMAX BLUE (PMS-281) RETURNS
- WHITE ACRYLIC FACES BLUE TRIM CAP
- GRIP-GARD SATIN FINISH CARMAX BLUE (PMS-281) WIREWAY

EXTERIOR WALL SIGNS



**YOUR IMAGE.
OUR PRIORITY.**

Architectural Graphics Inc.
2655 International Parkway
Virginia Beach, VA 23452

Phone: 757-427-1900
Fax: 757-427-6112

Revisions:

- 02/02/2018
- 04/05/2018
- 04/27/2018 ca
- 05/01/2018 ca
-
-

Account Rep:

Project Manager: **M.RHODES**

Drawn By: **JER**

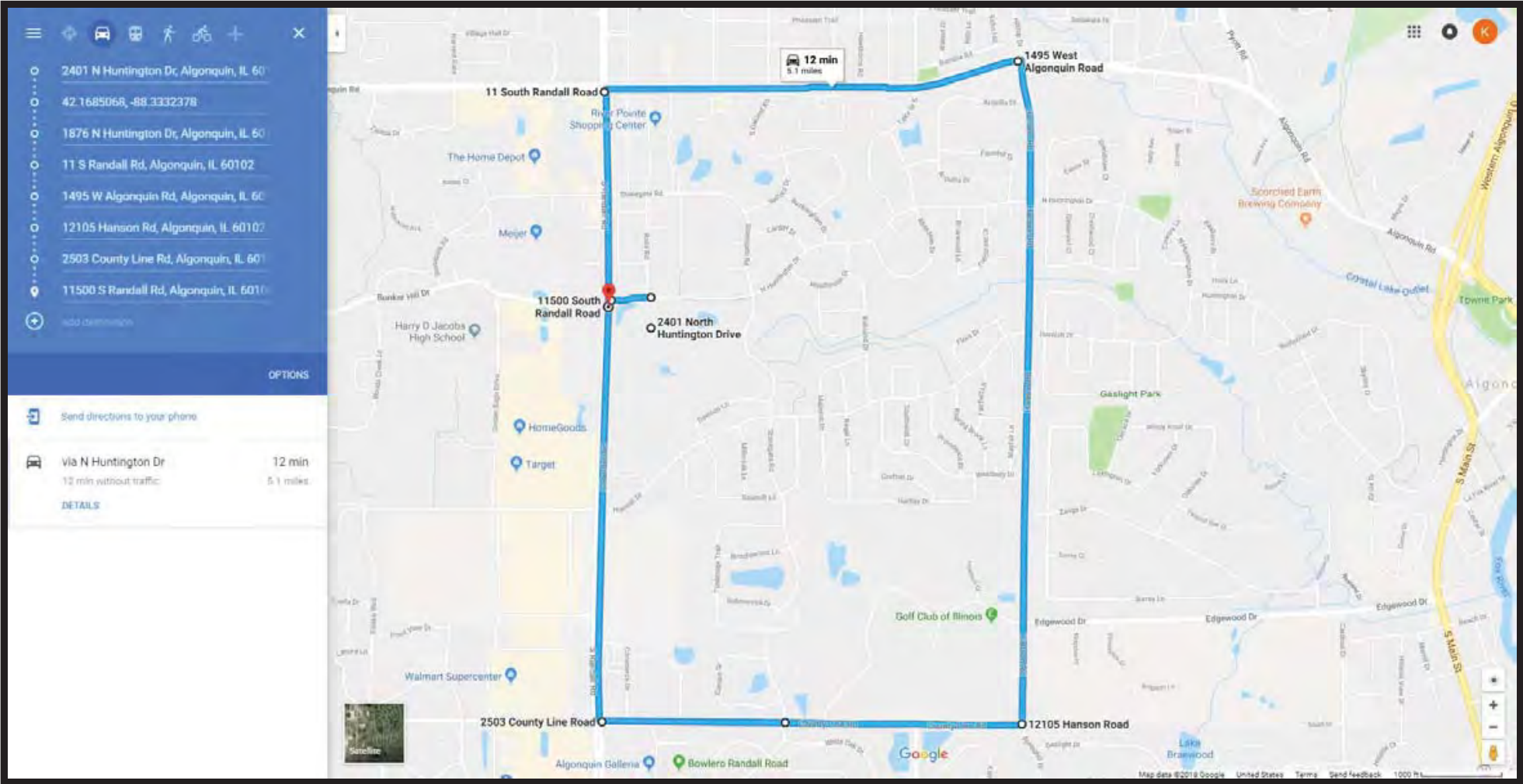
Project / Location:



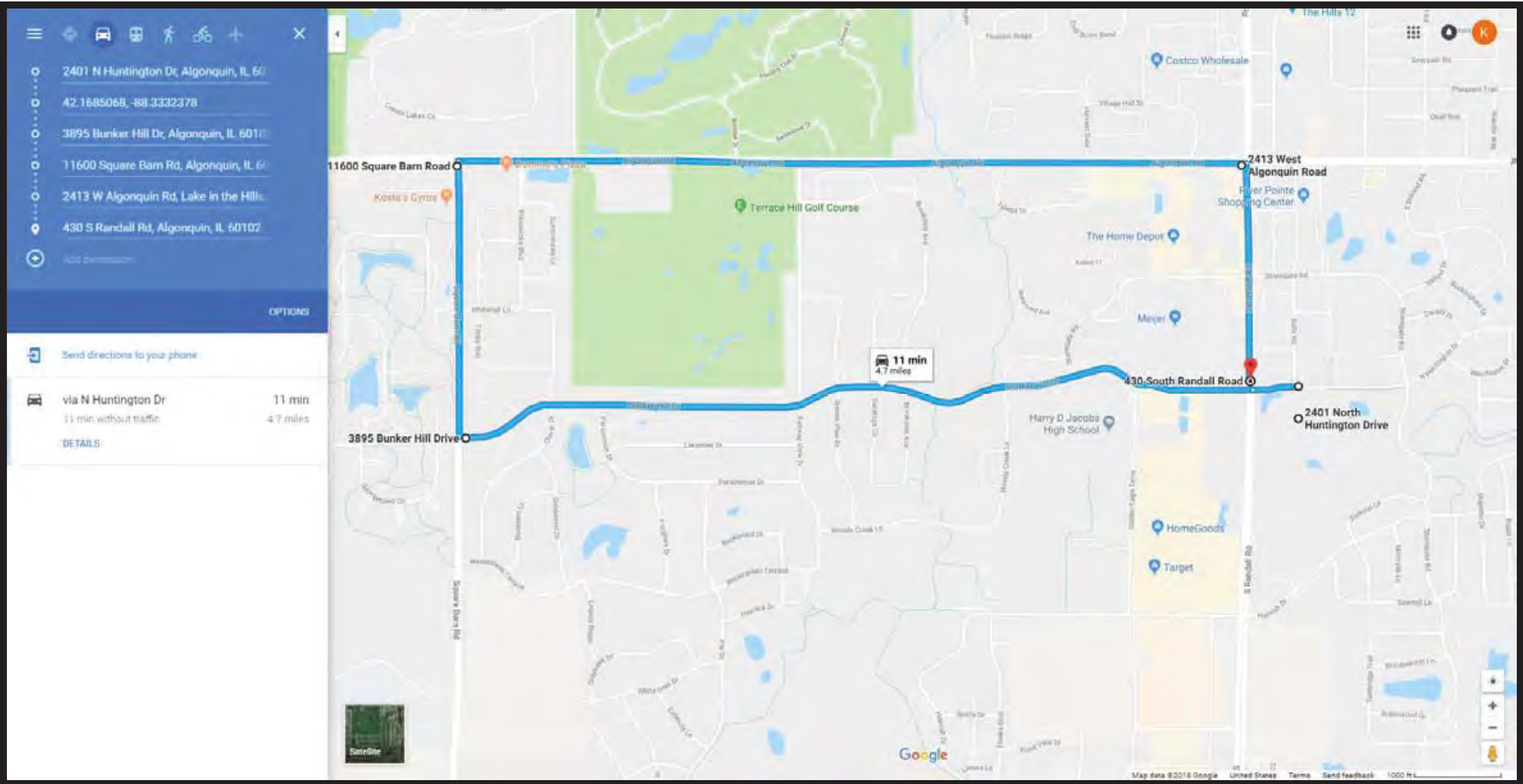
CARMAX # 6125
Site III Randall Rd
Algonquin, IL 60102

CarMax Algonquin Test Drive Route Alternatives (DRAFT)

Option 1:



Option 2:



BUSINESS DEVELOPMENT AGREEMENT

THIS BUSINESS DEVELOPMENT AGREEMENT (the "Agreement") is made and entered into as of this ____ day of _____, 2018, by and between THE VILLAGE OF ALGONQUIN, Kane and McHenry Counties, Illinois, an Illinois Home Rule municipal corporation duly organized and existing under the laws of the State of Illinois ("Village"), and CARMAX AUTO SUPERSTORES, INC., a Virginia corporation ("CarMax").

WITNESSETH:

WHEREAS, CarMax entered into an agreement to purchase certain real estate located within the corporate limits of the Village of Algonquin, Illinois, which real estate is legally described in Exhibit A attached hereto and made a part hereof, and which real estate is hereinafter referred to as the "Property"; and

WHEREAS, the Property is an industrially developed tract of land of approximately fifteen (15) acres, located at the southeast corner of Randall Road and Huntington Drive; and

WHEREAS, CarMax intends to redevelop a portion of the Property by constructing and operating a motor vehicle dealership selling and servicing primarily used automobiles on such portion of the Property (the "Project"), reserving the remaining portion of the Property for future development by CarMax or others (the "Surplus Property"); and

WHEREAS, the Village has determined that it is in the Village's best interest for CarMax to construct the Project in the Village of Algonquin; and

WHEREAS, the Village has determined that it is essential to the economic and social welfare of the Village that the Village promote the economic vitality of the community by assuring opportunities for development and sound and stable commercial growth within the corporate limits of the Village; and

WHEREAS, the Village, in adopting this Agreement, is exercising the powers provided by the Illinois statutes, specifically, 65 ILCS 5/8-11-20; and

WHEREAS, the Village has made the findings required by law and contained in 65 ILCS 5/8-11-20; specifically,

That the Property contains a structure that has remained significantly unoccupied or underutilized for a period of at least one (1) year; and

That the Project is expected to create and increase job opportunities within the Village; and

That the Project will serve to further the development of adjacent areas; and

That without the agreement, the Project would not be economically feasible; and

That CarMax meets high standards of creditworthiness and financial strength as demonstrated by one or more of the following:

1. Corporate debenture ratings of BBB or higher by Standard & Poor's Corporation or Baa or higher by Moody's Investors Service, Inc.;
2. A letter from a financial institution with assets of \$10,000,000 or more attesting to the financial strength of the developer; or
3. Specific evidence of equity financing for not less than 10% of the total project costs; and

That the Project will strengthen the commercial sector of the Village; and

That the Project will enhance the tax base of the Village; and

That this Agreement is made in the best interest of the Village; and

WHEREAS, the Village has further concluded that it may authorize this incentive under the authority of 65 ILCS 5/8-1-2.5 regarding expenses for economic development; and

WHEREAS, the Village, as a home rule unit of government is otherwise authorized to enter this Agreements and enact ordinances as the same pertains to its government and affairs (ILL.CONST. Art VII, §6); and

WHEREAS, the Village desires to have CarMax develop and operate the Project on the Property, as such will help to improve the social and economic welfare of the Village; and

WHEREAS, to make it economically feasible for CarMax to purchase the Property and to construct the Project thereon, the Village has agreed to offset a portion of the on-site and off-site development costs of the Project through the use of a rebate of a portion of the Sales Tax Revenue generated by CarMax within the corporate limits of the Village. The term "Sales Tax Revenue," as used herein, shall mean the Village's share of taxes imposed by the State as a Retailers' Occupation Tax, or any other sales tax or successor tax that may be enacted by the State as a replacement thereto, that are generated by the operation of the Project by CarMax on all or any portion of the Property, and distributed to the Village, and

WHEREAS, CarMax will be required to first comply with all planning and zoning regulations and apply for and receive preliminary and final PUD approval, and must construct and operate a bona fide car dealership prior to said tax rebate; and

WHEREAS, the Sales Tax Rebate (as defined in Section 5 below) is being provided by the Village to CarMax to provide CarMax with an incentive to develop the Project on the Property; and

WHEREAS, in exchange for the benefits derived from this Agreement, CarMax has agreed to redevelop the Property with certain upgraded design features including quality architecture; parking lot, perimeter and foundation landscaping; and tasteful signage, consistent with the plans approved by the Village Board as part of the final planned development; and

WHEREAS, the tax on the retail sales subject to this Agreement, absent the Agreement, would not have been paid to another unit of local government; and CarMax affirms that it does not maintain,

within another unit of local government, a retail location from which the tangible personal property to be sold at this motor vehicle dealership would have been delivered to purchasers, or a warehouse from which the tangible personal property would have been delivered to purchasers in the manner prohibited by 65 ILCS 5/8-11-21; and

WHEREAS, the President and Board of Trustees have determined that entering into this Agreement is in the best interests of the Village;

NOW THEREFORE, in consideration of the foregoing and the covenants hereinafter set forth, it is mutually agreed as follows:

1. Recitals: The recitals set forth hereinabove are hereby incorporated by reference, as if fully set forth herein.

2. Legislative Authority: The Village made the findings of fact required by 65 ILCS 5/8-11-20. The Village represents that the adoption of this Agreement is within its scope of authority pursuant to statute and home rule authority, and that it is duly authorized and empowered to enter into and carry out the terms of this Agreement.

3. Parties to the Agreement: This Agreement is made by and between CarMax and the Village as the parties to the Agreement. In the event that CarMax ceases to operate the Project on the Property at any time during the term of this Agreement for a period of longer than six (6) months, other than in connection with remodeling or reconstructing the Project or for reasons not within the reasonable control of CarMax, the Village shall have no further obligations under this Agreement.

4. Responsibilities of CarMax: CarMax agrees to diligently undertake the following:

A. CarMax shall complete the purchase of the Property by no later than December 31, 2018;

B. CarMax shall apply for and obtain demolition, site development and building permits and commence demolition, site work and other construction necessary to establish the Project on the Property by no later than _____;

C. CarMax shall diligently prosecute construction of same, and shall obtain an occupancy permit by no later than _____;

D. CarMax agrees to construct all Project improvements in substantial accordance with Village codes, plans and specifications to be submitted and approved by the Village, and CarMax shall operate its business on the Property in accordance with all Village codes and all applicable laws and regulations of the Village, the State of Illinois and the United States government.

5. Sales Tax Rebate: Subject to compliance with the terms of this Agreement, CarMax shall be entitled to receive, on a quarterly basis, a sales tax rebate payment as follows (the "Sales Tax Rebate"):

A. Eligibility for the Sales Tax Rebate, as outlined further below, shall begin upon CarMax's completion of and occupancy (by evidence of a temporary or final certificate of occupancy) of the Project on the Property (the "Commencement Date");

B. The Village shall retain fifty percent (50%) of all Sales Tax Revenue generated by the Project at the Property and shall pay a Sales Tax Rebate to CarMax of the remaining fifty percent (50%) of such Sales Tax Revenue generated by the Project for a ten (10) year period after the Commencement Date or until such time as the total Sales Tax Rebate paid to CarMax by the Village equals or exceeds the Rebate Cap (as defined in Section 6 below), whichever occurs first (such period of time is hereinafter referred to as the "Rebate Term"). The Village shall have no obligation to pay any funds to CarMax from any source other than from the Municipal Retailers' Occupation Sales Taxes generated by the Project at the Property and actually received by the Village from the Illinois Department of Revenue.

C. CarMax shall provide the Village with a written authorization enabling the Village to obtain certified reports from the Illinois Department of Revenue (such as the Illinois Department of Revenue document "Authorization to Release Sales Tax Information to Local Governments" or any other such authorization necessary to enable the Village to obtain certified reports from the Illinois Department of Revenue), verifying the taxable sales by CarMax for each calendar quarter during the Rebate Term. CarMax represents to the Village that it currently files all records pertaining to its business in the State of Illinois with the Illinois Department of Revenue electronically, and CarMax agrees to continue to file all such records electronically during the Rebate Term.

D. In the event the Commencement Date is within calendar year 2020, the Village agrees to provide an additional sales tax rebate of twenty-five percent (25%) of any sales occurring in 2020 (bringing CarMax share to 75%), and an additional ten percent (10%) of any sales occurring in 2021 (bringing CarMax share to 60%), however, in no case shall the total rebate exceed the Maximum Rebate Amount.

6. Maximum Rebate Amount: Notwithstanding anything in this Agreement to the contrary, the combined total of the Sales Tax Rebate amounts payable to CarMax by the Village under this Agreement shall not exceed \$ 800,000.00 (the "Rebate Cap").

7. Term and Timeline of Rebate Payments: The obligations hereunder shall expire upon the expiration of the Rebate Term (the "Termination Date"), after which the Village shall have no further obligations to share or rebate Sales Tax Revenue under the terms of this Agreement (other than Sales Tax Rebates arising from taxable events during the Rebate Term, which have not yet

been paid to CarMax prior to the Termination Date, which obligation shall survive the expiration of this Agreement). Payment to CarMax shall be made on a quarterly basis, within sixty (60) days from and after the Village's actual receipt of the Village's distributive share of Sales Tax Revenue. The Village shall provide CarMax with a quarterly report of all Sales Tax Revenue generated by CarMax and actually received by the Village for the preceding quarterly period.

8. Sales Tax Reports: The parties acknowledge and agree that the Village shall use commercially reasonable efforts to obtain the Sales Tax Revenue information pertaining to the Project directly from the Illinois Department of Revenue pursuant to the authorization from CarMax contemplated by Section 5C above. To the extent that such information is not available to the Village, the Village shall notify CarMax of such unavailability, and CarMax agrees that, within seventy-five (75) days after the end of each quarter of each calendar year of the Rebate Term, CarMax shall provide the Village with a statement, accompanied by a summary of the Gross Receipts pertaining to the Project for such calendar year (or the portion of any partial calendar year falling within the Rebate Term), in a form reasonably acceptable to the Village, signed by an officer of CarMax, which shall set forth the dollar amount of sales taxes paid to the State of Illinois for the benefit of the Village during the prior calendar year. The term "Gross Receipts," as used herein shall have the same meaning as that which is ascribed to it in the Retailer's Occupation Tax Act. Additionally, CarMax shall maintain and have available for inspection by the Village copies of any and all sales tax returns, sales tax reports, amendments, proof of payment or any other Sales Tax information filed with the State of Illinois or other appropriate governmental entity, to the extent relating to the Project, which documents are being held for and shall be made available to the Village for purposes of identifying Sales Tax Revenue collected pursuant to this Agreement. Additionally, the Village shall use its best efforts to obtain such Sales Tax information directly from the Illinois Department of Revenue of the State of Illinois.

9. Limitation of Village Responsibilities: Other than the Sales Tax Rebate pursuant to this Agreement, the Village shall have no obligation to incur any expense associated with the construction and completion of the Project.

10. Confidentiality of Financial Information: To the extent permitted by law, the Village shall endeavor to maintain the confidentiality of the information contained in any financial reports submitted by CarMax, the Gross Receipts and other financial information of CarMax received by the Village, but shall be permitted to disclose such information and documents to employees and consultants that the Village (except for any consultant that has a financial interest in any motor vehicle dealership within the Chicago metropolitan area, which consultant shall not be retained), in its sole discretion, deems appropriate in order to monitor compliance and audit this Agreement. CarMax understands and agrees that the provisions of this Agreement shall be a matter of public record, as shall any and all payments to CarMax pursuant to this Agreement.

11. Mutual Assistance: The Village and CarMax agree to do all things reasonably necessary or appropriate to carry out the terms and provisions of this Agreement and to reasonably aid and assist each other in carrying out the terms hereof.

12. Provisions Concerning Limitation on Debt: The Village's receipt, or anticipated receipt, of Sales Tax Revenue, either directly or indirectly, from CarMax, as provided earlier in this Agreement, shall be a condition precedent to any obligation of the Village to pay monies to CarMax.

13. Governing Law, Waiver and Notices: This Agreement shall be governed by the laws of the State of Illinois, and the sole and exclusive venue for any disputes arising out of this Agreement shall be the Twenty-Second Judicial Circuit, McHenry County, Illinois. A waiver of any part of this Agreement shall be limited to that specific event and shall not be a waiver of the entire Agreement. Any notices required in this Agreement shall be effective when in writing and three (3) days after mailing by certified mail, return receipt requested, or by delivering the same in person or to any officer of such party or by prepaid telegram or private overnight courier, when appropriate, addressed to the party to be notified.

All notices to the Village shall be sent to:

Village Manager
Village of Algonquin
2200 Harnish Drive
Algonquin, IL 60102

With a copy to :
Village Attorney
Kelly A. Cahill
Zukowski, Rogers, Flood & McArdle
50 Virginia St.
Crystal Lake, IL 60014

All notices to CarMax shall be sent to:

CarMax Auto Superstores, Inc.
12800 Tuckahoe Creek Parkway
Richmond, VA 23238

Attention: Real Estate Department

With a copy to:

CarMax Auto Superstores, Inc.

P.O. Box 29965
Richmond, VA 23242
Attention: Tax Department

or to such other address as a party may designate for itself by notice given from time to time to the other parties in the manner provided herein.

14. Time is of the Essence: Time is of the essence of this Agreement. The parties shall make every reasonable effort to expedite the subject matters hereof, and they hereby acknowledge that the successful performance of this Agreement requires their continued cooperation.

15. Remedies: The failure by CarMax to comply with any material term, provision, or condition of this Agreement within the times herein specified in any material respect after notice and an opportunity to cure shall constitute an event of default. Upon the occurrence of an event of default by CarMax, the Village shall provide written notice to CarMax specifying the nature of the default. Upon receipt of such notice of default, CarMax shall have ninety (90) days to cure the default, if it can reasonably be cured within ninety (90) days, or must begin curing the default within a reasonable time thereafter, if it cannot be cured within ninety (90) days. If a default by CarMax is not cured as provided above, the Village may withhold payment of any Sales Tax Rebate that may be due, until CarMax establishes compliance with the terms of this Agreement, or the Village may terminate this Agreement, as its sole and exclusive remedies, and, in the event of a termination by the Village, the Village shall be relieved of any further obligations arising pursuant to this Agreement, and CarMax shall be relieved from its obligations hereunder.

The Village shall be considered in default of this Agreement if it shall fail to make complete and timely payments to CarMax on the terms provided herein or fail to comply with any other material term or condition of this Agreement. In such event, CarMax shall notify the Village of the default in writing, and the Village shall have a period of ninety (90) days thereafter to cure such default. The Village's failure to cure the default shall relieve CarMax of its obligations under this Agreement, but CarMax shall not be deemed thereby to have waived its rights to seek remedies at law or in equity, including, but not limited to, damages and specific performance provided that the Village shall not be liable for any consequential damages.

This Paragraph shall not apply for issues related to *force majeure* or other circumstances beyond the reasonable control of CarMax, provided that CarMax has taken reasonable actions and made commercially reasonable, good faith efforts to otherwise comply with the deadlines set forth herein.

16. Destruction of Improvement: In the event that the Project improvements on the Property shall be destroyed by an act of God, natural causes or catastrophe, prior to the full payout of the Sales Tax Rebate pursuant to the terms herein, then the obligation of the Village to share Sales Tax Revenue with CarMax shall be extended for a period of time equal to the lesser of the following:

A. The interval commencing on the date of the destruction of the improvements and ending on the date a new certificate or occupancy for the rebuilt improvement is issued; or

B. Twelve (12) months.

17. Entire Agreement: This instrument contains the entire agreement between the parties with respect to the transaction contemplated in this Agreement.

18. Severability: If any provision of this Agreement is held invalid by a court of competent jurisdiction, after exhaustion of all appeals or periods for such, or in the event such a court shall determine that the Village does not have the power to perform any such provision, after exhaustion of all appeals or periods for such, such provision shall be deemed to be excised herefrom, and the invalidity thereof shall not affect any of the other provisions contained herein, and such judgment or decree shall relieve the Village from performance under such invalid provision of this Agreement; provided, however, if the judgment or decree relieves the Village of any of its monetary obligations under Section 6 of this Agreement, then this Agreement shall terminate.

19. Amendment: This Agreement may not be amended, altered or revoked at any time, in whole or in part, unless such changes are agreed to in writing and signed by all of the parties to this Agreement.

20. Binding On: This Agreement shall be binding on the parties, and their respective successors, assigns, heirs and legal representatives.

21. Section and Other Headings: Section or other headings contained in this Agreement are for reference purposes only and shall not affect in any way the meaning or interpretation of this Agreement.

22. Authorization to Execute: The officer of CarMax who has executed this Agreement warrants that he has been lawfully authorized by the Board of Directors of CarMax to execute this Agreement on its behalf. The President and Clerk of the Village hereby warrant that they have been lawfully authorized by the Village Board of Algonquin to execute this Agreement. CarMax and the Village shall deliver, upon request, to each other at the respective time such entities cause their authorized agents to affix their signatures hereto, copies of all articles of incorporation, by-laws, resolutions, ordinances or other documents required to legally evidence the authority to so execute this Agreement on behalf of the respective parties.

23. Counterparts: This Agreement may be executed in multiple counterparts, all of which shall be considered one (1) and the same agreement. Copies of the executed counterparts transmitted by electronic transmission shall be considered original executed counterparts.

24. Surplus Property: For the avoidance of doubt, this Agreement does not include any sales activity that might be conducted on the Surplus Property, unless CarMax expands the Project to the Surplus Property and the Village has received sales tax revenue from the Illinois Department of Revenue for CarMax sales on that Surplus Property.

[SIGNATURE PAGE FOLLOWS]

DRAFT

IN WITNESS WHEREOF, the parties hereto have executed this Agreement on the date first above written.

CARMAX AUTO SUPERSTORES, INC.,
a Virginia corporation

By: _____
K. Douglass Moyers
Vice President, Real Estate

WITNESS for CarMax:

By: _____
_____, its _____

VILLAGE OF ALGONQUIN,
an Illinois Home Rule municipal corporation

By: _____
John Schmitt, President

ATTEST:

By: _____
Gerald Kautz, Village Clerk